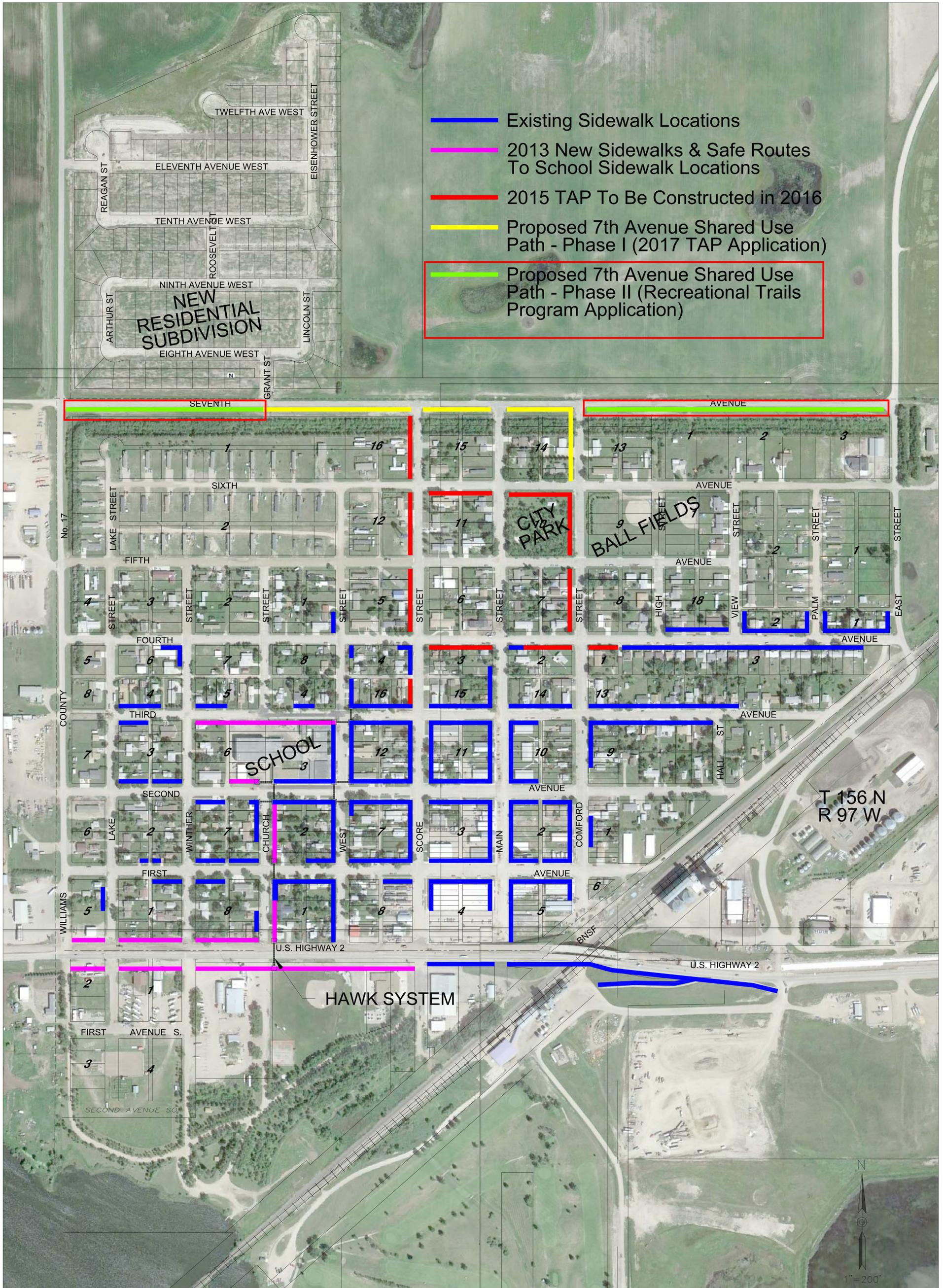


Project Location Map

RAY, NORTH DAKOTA IN WILLIAMS COUNTY
2017 TRANSPORTATION ALTERNATIVE PROGRAM & RECREATIONAL TRAILS PROGRAM
7TH AVENUE SHARED USE PATH PHASE I & II







Proposed Alignment between trees and power poles

05/09/2014



Picture showing typical clearing and grubbing required along proposed alignment

05/09/2014

CONSTRUCTION PLANS FOR 7TH AVENUE SHARED USE PATH

RAY, NORTH DAKOTA

PREPARED FOR
CITY OF
RAY, NORTH DAKOTA

DESIGN CRITERIA

DESIGN SPEED: 20 MPH
LEAN ANGLE: 15 DEGREES
WIDTH: 10 FEET
CROSS SLOPE: 2 PERCENT

APPROVED : _____, 2014
BY: INTERSTATE ENGINEERING, INC.
PROJECT ENGINEER

QUALITY REVIEW BY: _____, 2014
BY: INTERSTATE ENGINEERING, INC.
PROJECT MANAGER

NOT APPROVED

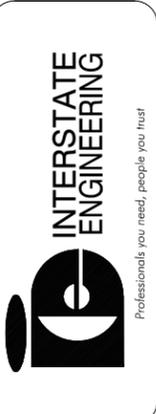
INDEX OF DRAWINGS

SEQUENCE NUMBER	SHEET NUMBER	SHEET TITLE
1	G-1	COVER SHEET
2	G-2	GENERAL NOTES
3	G-3	CONTROL
4	D-1	DETAILS
MULTI-USE PATH		
5	C-1	0+00 TO STA 5+00
6	C-2	5+00 TO STA 10+00
7	C-3	10+00 TO STA 15+00
8	C-4	15+00 TO STA 20+00
9	C-5	20+00 TO STA 25+00
10	C-6	25+00 TO STA 30+00
11	C-7	30+00 TO STA 35+00
12	C-8	35+00 TO STA 40+00
13-17	X-1TOX-5	CROSS SECTIONS

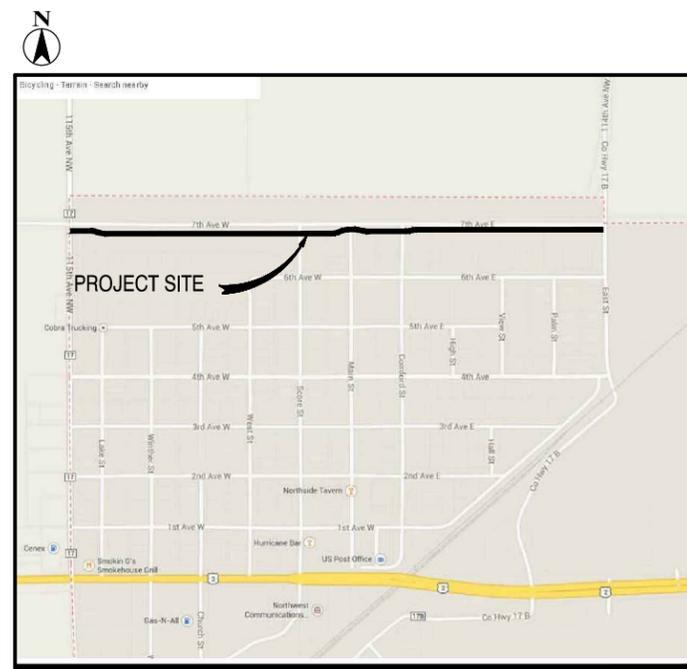
DRAWN BY: V.M.M.
CHECKED BY: C.C.C.
DATE: 10/14
JOB NO. S14-00-076
REVISIONS:

7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
COVER

Interstate Engineering, Inc.
P.O. Box 20953
1211 Grand Avenue Suite 6
Billings, Montana 59104
Ph (406) 256-1920
Fax (406) 256-9178
www.interstateeng.com
Other offices in Minnesota, North Dakota and South Dakota



LOCATION MAP
NTS



SITE MAP
NTS

NOT APPROVED

LEGEND

<u>EXST</u>	<u>ABBR</u>	<u>NEW</u>	<u>UTILITY</u>
	MH		SEWER MANHOLE (48")
	FH		FIRE HYDRANT
	W V		WATER VALVE BOX AND RISER
	PP		POWER POLE
	XFMR		TRANSFORMER
			TELEPHONE PEDISTAL
			<u>SURVEY</u>
	CP-1		SURVEY CONTROL
<u>EXST</u>	<u>NEW</u>	<u>LINETYPE</u>	
			WATER MAIN & SIZE
			SEWER MAIN & SIZE
			STORM DRAIN PIPE & SIZE
			GAS LINE
			ELECTRIC LINE
			ROADWAY CENTERLINE
			RIGHT OF WAY
			PROPERTY LINE
			PUBLIC UTILITY EASEMENT
			TEMPORARY CONSTRUCTION EASEMENT
			INDEX CONTOUR
			INTERMEDIATE CONTOUR
			TELCO LINE (UG)
			FENCE
			CUT LIMITS
			FILL LIMITS
			SIGN
			MAIL BOX
			FLAG POLE

GENERAL NOTES:

- Project control/benchmarks are as shown on these plans.
- Governing specification for construction is the AASHTO Guide for the Development of Bicycle Facilities, Fourth Edition, 2012.
- Utility information shown on plans was obtained from a combination of field survey, existing mapping, as-built construction drawings or utility locates. The contractor shall verify the depth of existing utilities where new construction crosses existing utilities.
- Contractor shall call for utility locates prior to beginning any work. Contractor shall protect all underground utilities. Contractor shall coordinate any required utility relocations with affected utilities.
- Contractor is responsible for developing the Storm Water Pollution Prevention Plan (SWPPP) and for filing the Notice of Intent with the North Dakota Department of Health for this project.
- All embankment fills within the street right-of-way shall be compacted to 95% maximum dry density as determined by ASTM D698, and shall be within 3% of optimum moisture content. Any trench backfill within the street right-of-way shall be compacted to 97% maximum dry density as determined by ASTM D698, and shall be within 3% of optimum moisture.
- All disturbed areas shall be re-seeded by broadcasting at a rate of 30 pounds per acre. Seed shall consist of the following mix, or approved equal:
 - 40% Fairway Crested Wheatgrass
 - 30% Perennial Ryegrass
 - 20% Smooth Brome
 - 10% Canadian Bluegrass
- Areas to be reseeded shall have at least 6" of suitable topsoil cover. On-site topsoil disturbed by construction shall be stockpiled and utilized to meet this requirement. Additional imported topsoil may also be required.
- Disturbed slopes steeper than 4:1 (horizontal: vertical) and constructed drainage ditch sections steeper than 4% shall be covered with North American Green SC150BN Biodegradable Erosion Control Blanket or approved equal. Erosion control blanket shall be installed in accordance with manufacturer's instructions.
- The gradation of Aggregate Base Course shall conform to the requirements for Class 5 Aggregate as specified in "Standard Specifications for Road and Bridge Construction", 2008 Edition as published by the NDDOT, and all supplemental specifications.
- Surface course asphalt aggregate shall conform to the requirements for Class 29 Aggregate as specified in the "Standard Specifications for Road and Bridge Construction", 2008 Edition as published by the NDDOT and all supplemental specifications.
- Riprap shall meet the specifications of the "Standard Specifications for Road and Bridge Construction", 2008 Edition as published by the NDDOT and all supplemental specifications.
- Granular bedding material for use under riprap shall meet the "Standard Specifications for Road and Bridge Construction", 2008 Edition as published by the NDDOT and all supplemental specifications.
- Separation fabric shall be Propex 701 or approved equal.
- RCP Culvert Pipe shall be Class III with minimum Wall B.
- C.S.P. pipe and end sections shall meet the specifications of Section 709.02 of the MDT Standard Specifications for Road and Bridge Construction. All C.S.P.'s shall have flared end sections.
- Channel shaping for transitions to drainage structures shall result with side slopes no steeper than 2:1 (horizontal:vertical).
- Path signs shall be installed in accordance with Chapter 9 of the Manual on Uniform Traffic Control Devices, 2009 Edition. Sheet aluminum for signs shall meet the requirements of Section 704.01.1 of the MDT Standard Specifications for Road and Bridge Construction. Sign posts shall be steel U-posts meeting the requirements of Section 704.01.4 of the MDT Standard Specifications for Road and Bridge Construction.
- Items not included as a pay item on the bid form shall be provided by the contractor at no additional cost to the owner and shall be considered incidental items.

Revision No.	Date	By	Description

7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
GENERAL NOTES

Project No.: S14-00-076
 Date: 10/14

Drawn By: V.M.M.
 Checked By: C.C.C.

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

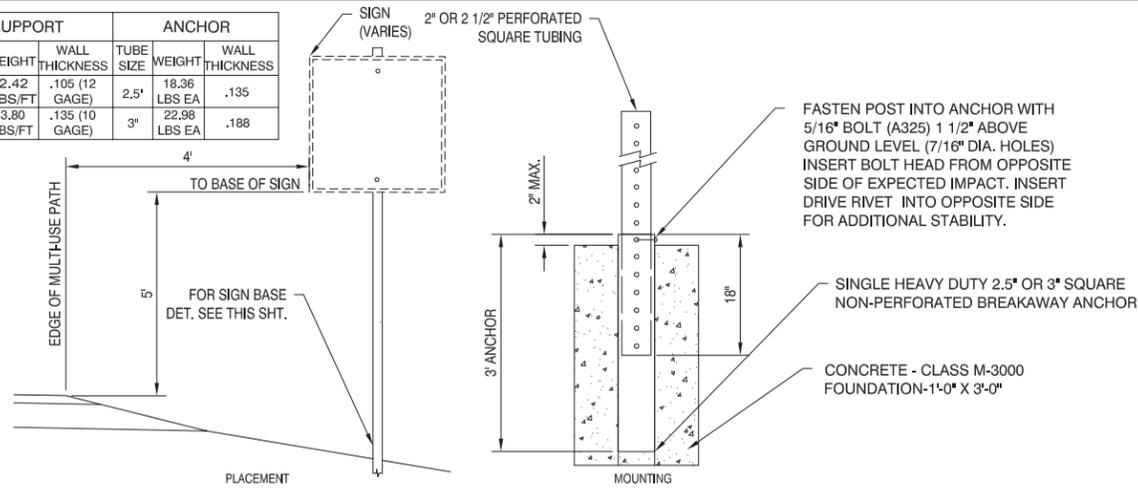
Other offices in Minnesota, North Dakota and South Dakota

INTERSTATE ENGINEERING
 Professionals you need, people you trust!



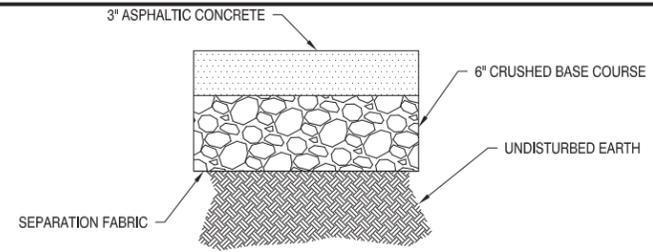
C:\Users\Veronica.Meyer\Desktop\S14-00-076\CAD\DESIGN\SHEETS\G-2.dwg 10/28/2014 2:38:54 PM

SUPPORT			ANCHOR		
TUBE SIZE	WEIGHT	WALL THICKNESS	TUBE SIZE	WEIGHT	WALL THICKNESS
2"	2.42 LBS/FT	.105 (12 GAGE)	2.5"	18.36 LBS EA	.135
2.5"	3.80 LBS/FT	.135 (10 GAGE)	3"	22.98 LBS EA	.188

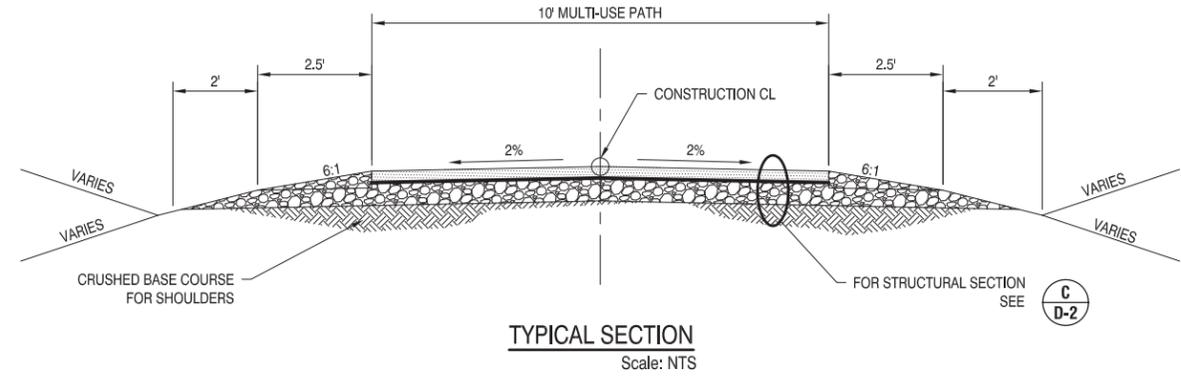


A SIGN BASE PLACEMENT & MOUNTING DETAIL
Scale: NTS

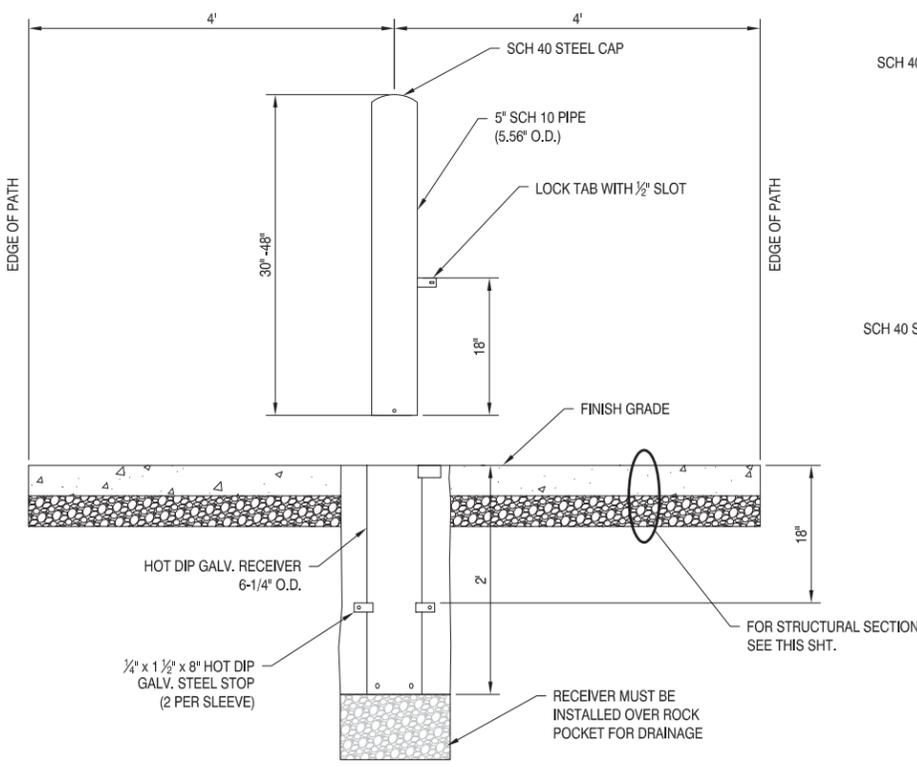
- NOTE:
1. ALTERNATE CRASH TESTED AND APPROVED BREAKAWAY DEVICES MAY BE USED AS AN OPTION WITH PRIOR APPROVAL OF THE ENGINEER.
 2. USE CONCRETE CLASS "A" OR "D" WITH WOOD FLOAT FINISH ON TOP. FORM TOP 6" OF FOUNDATION.
 3. GALVANIZED PIPE PER AASHTO M 111.
 4. PAINT PIPE WITH ONE SHOP COAT AND ONE FIELD COAT OF ZINC RICH BASED PAINT AS SPECIFIED IN THE MONTANA DEPT. OF TRANSPORTATION STANDARD SPECIFICATIONS SECTION 710, ON ALL SURFACES NOT IN CONTACT WITH CONCRETE.
 5. CONFORM STEEL PIPE TO THE REQUIREMENTS OF ASTM A 53 TYPE "E" OR "S", GRADE "B".
 6. SUBMIT SHOP DRAWINGS TO BE APPROVED BY THE ENGINEER BEFORE FABRICATION HAS BEGUN.
 7. * SHIMS ARE NOT TO BE USED.



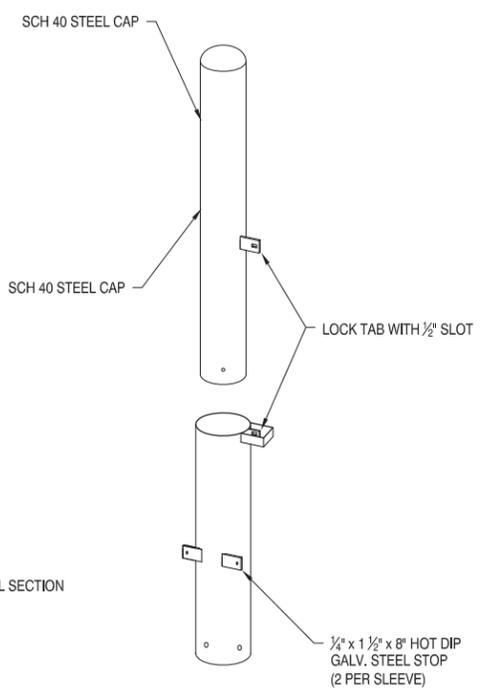
C MULTI-USE PATH STRUCTURAL SECTION
Scale: NTS



TYPICAL SECTION
Scale: NTS



B REMOVABLE BOLLARD DETAIL
Scale: NTS

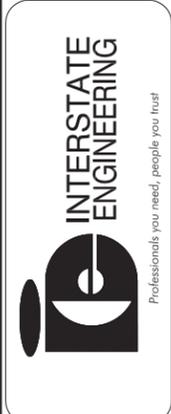


NOT APPROVED

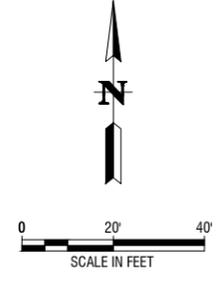
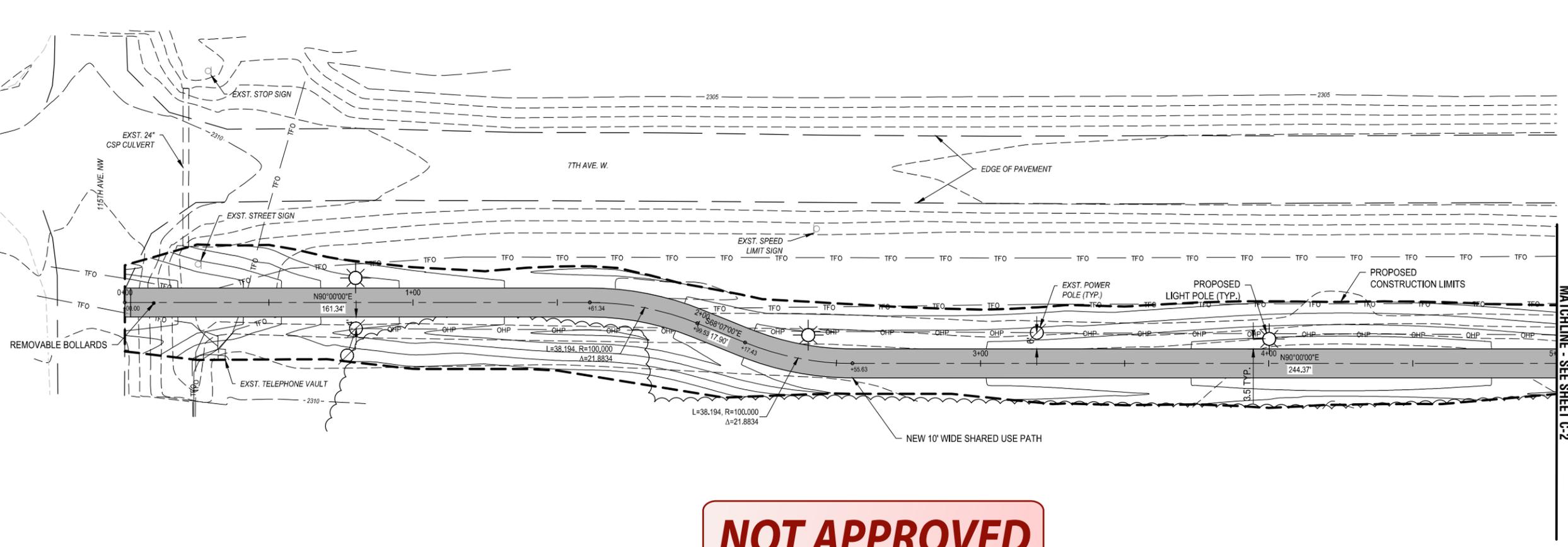
Revision No.	Date	By	Description

7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
SHARED USE PATH
DETAILS
Project No.: S14-00-076
Date: 10/14
Drawn By: V.M.M.
Checked By: C.C.C.

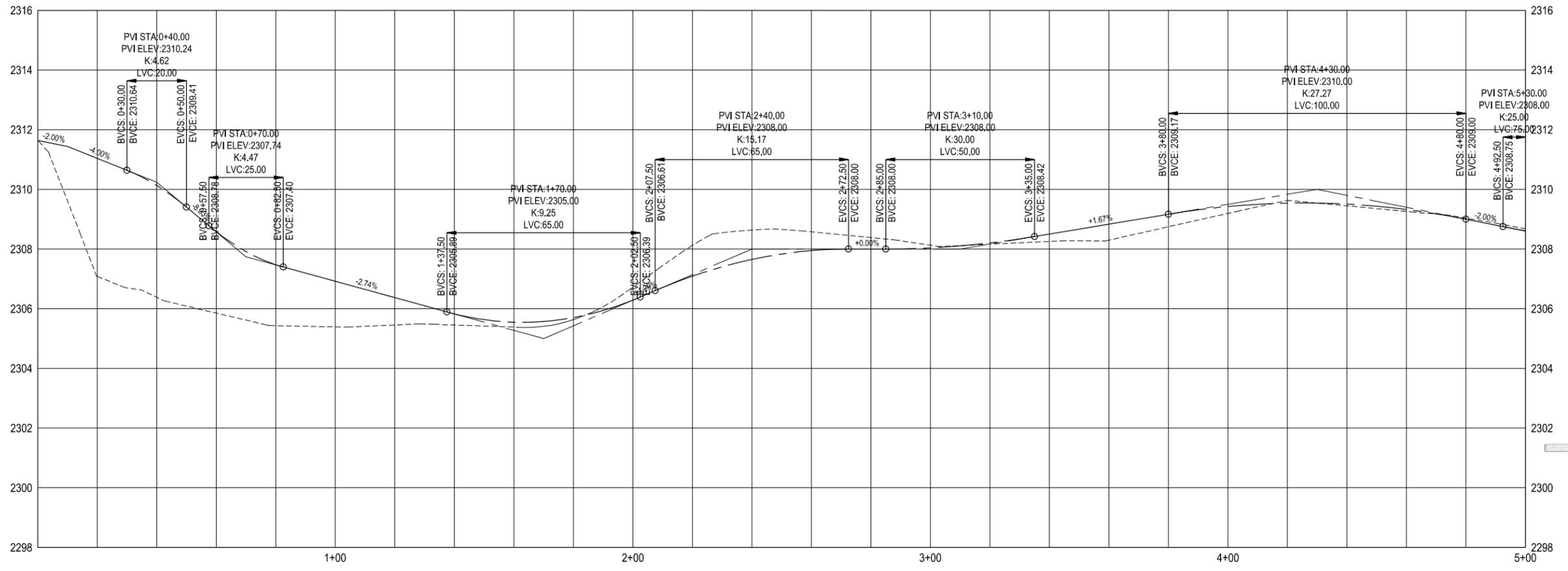
Interstate Engineering, Inc.
P.O. Box 20953
1211 Grand Avenue Suite 6
Billings, Montana 59104
Ph (406) 256-1920
Fax (406) 256-9178
www.interstateeng.com
Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Meyer\Desktop\S14-00-076\CAD\DESIGN\SHEETS\D-1.dwg 10/28/2014 3:02:18 PM



NOT APPROVED



C:\Users\Veronica.Moya\Documents\S14-00-076\CAD\DESIGN\SHEETS\C-1.dwg 10/28/2014 2:39:47 PM

Revision No.	Date	By	Description

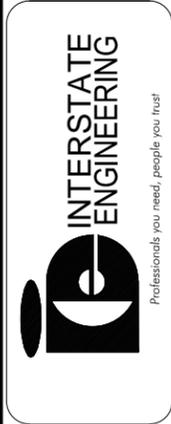
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

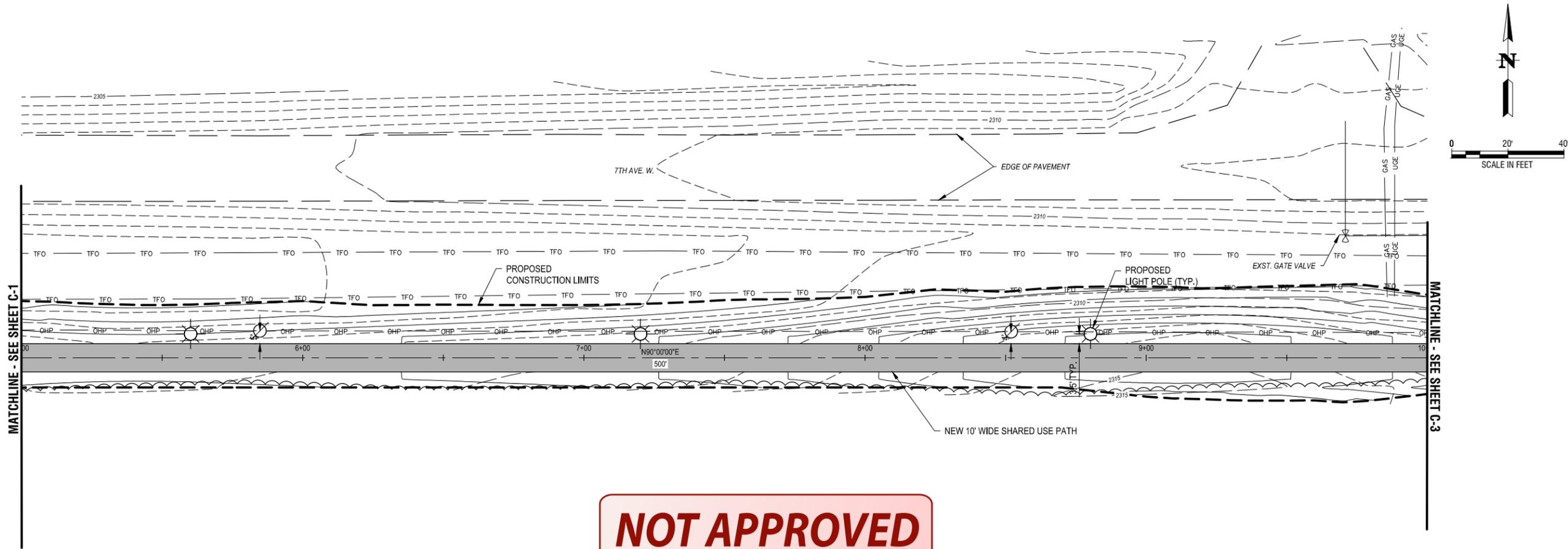
Project No.: S14-00-076
 Date: 10/14

Drawn By: V.M.M.
 Checked By: C.C.C.

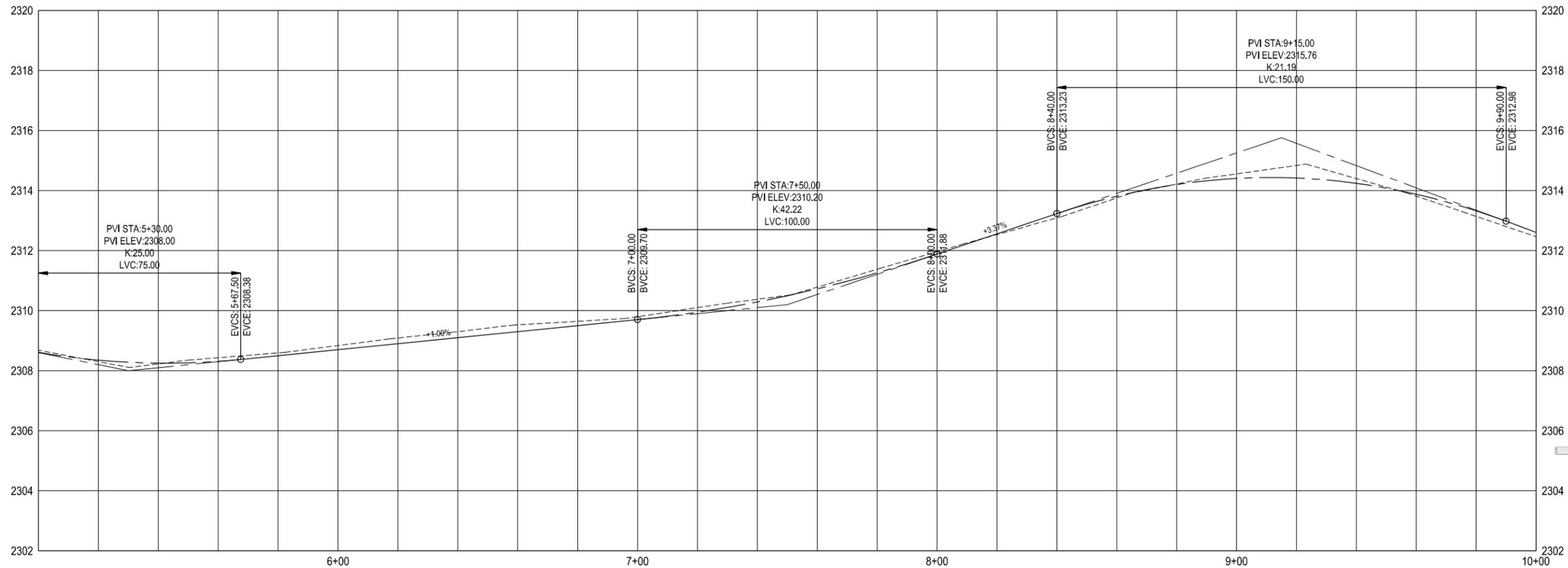
Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

Other offices in Minnesota, North Dakota and South Dakota





NOT APPROVED



Revision No.	Date	By	Description

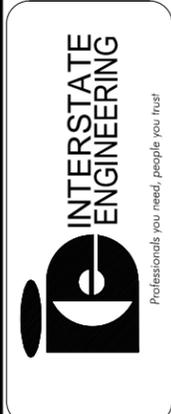
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

Drawn By: V.M.M.
 Checked By: C.C.C.

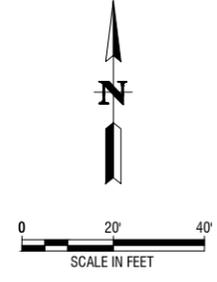
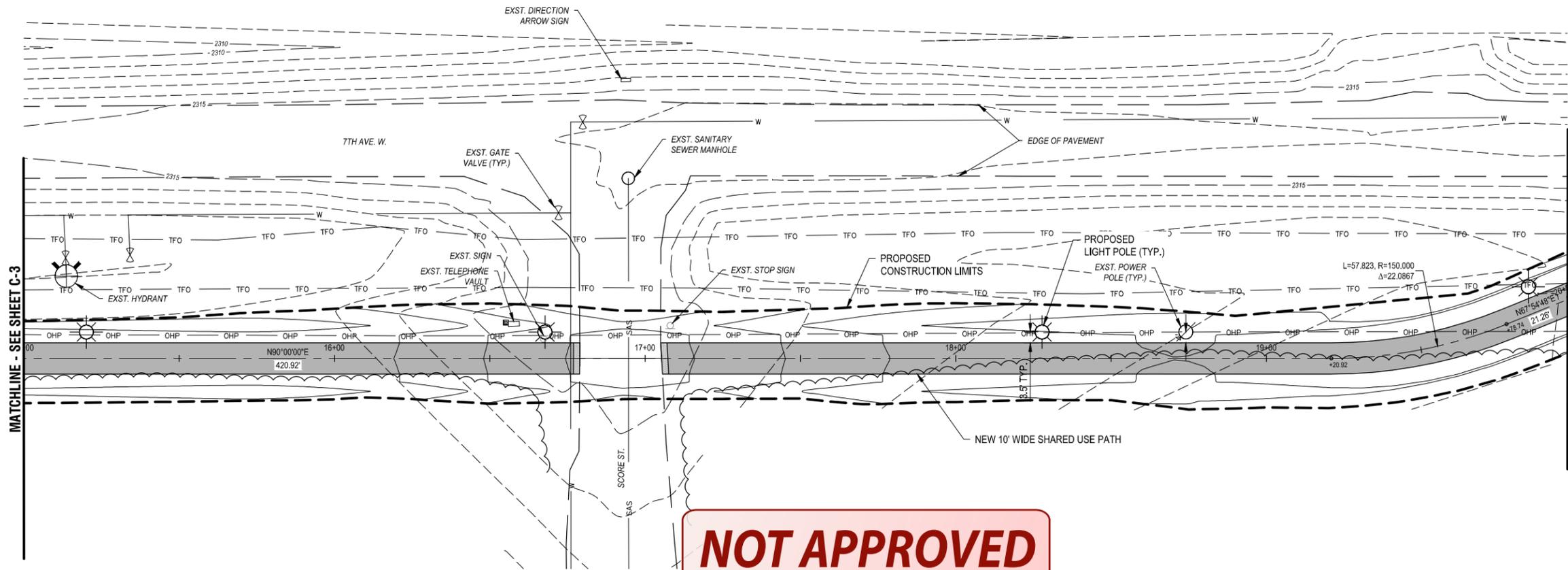
Project No.: S14-00-076
 Date: 10/14

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

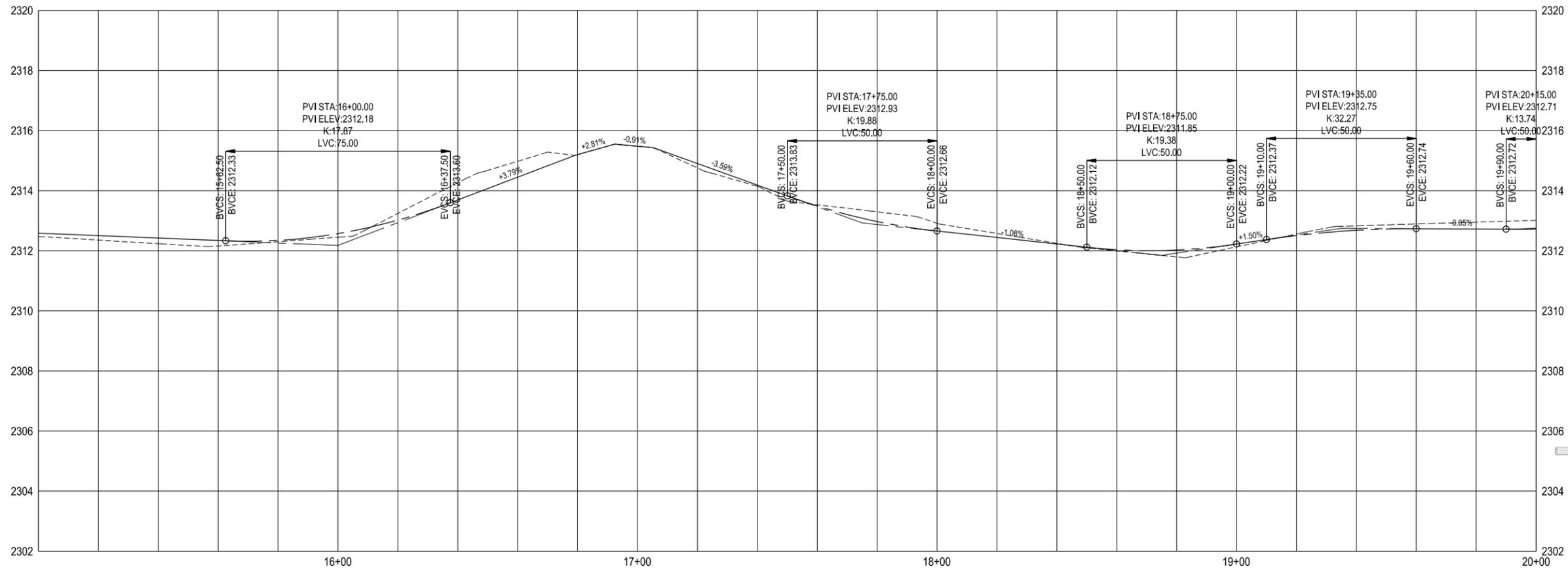
Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Moya\Desktop\S14-00-076\CAD\DESIGN\SHEETS\C-2.dwg 10/28/2014 2:40:15 PM



NOT APPROVED



Revision No.	Date	By	Description

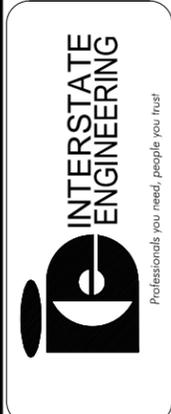
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

Drawn By: V.M.M.
 Checked By: C.C.C.

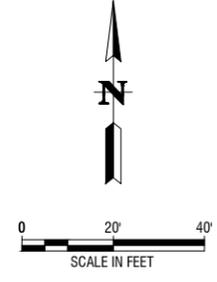
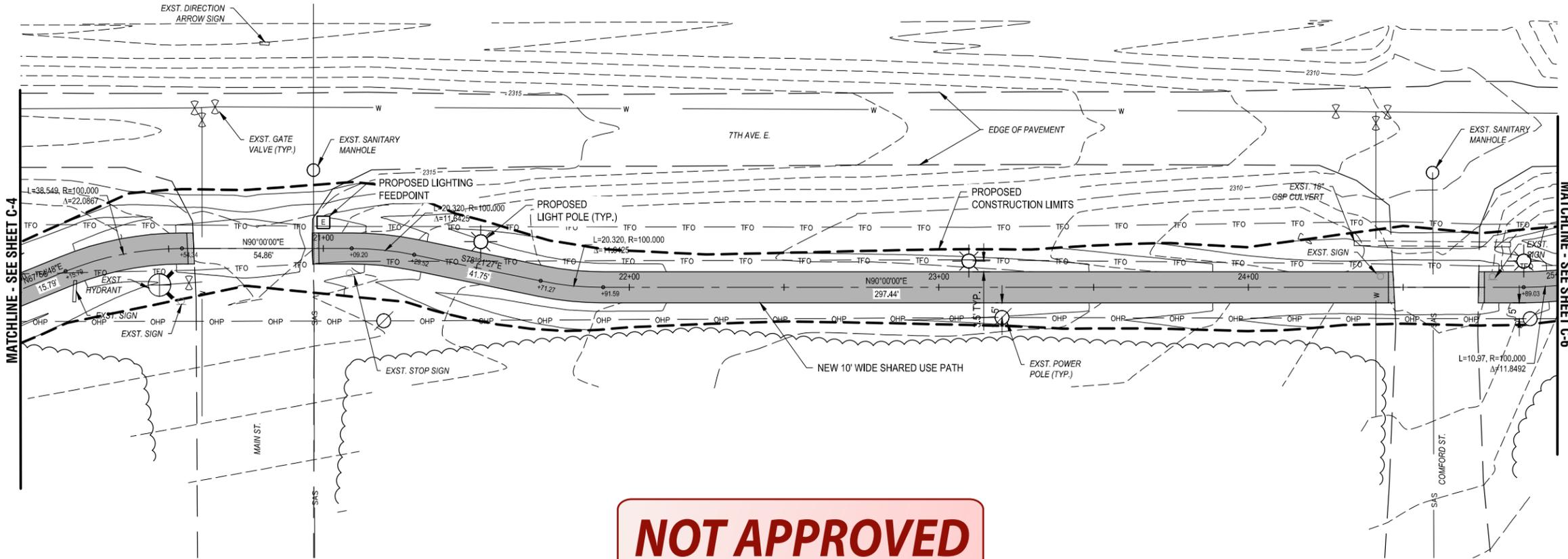
Project No.: S14-00-076
 Date: 10/14

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

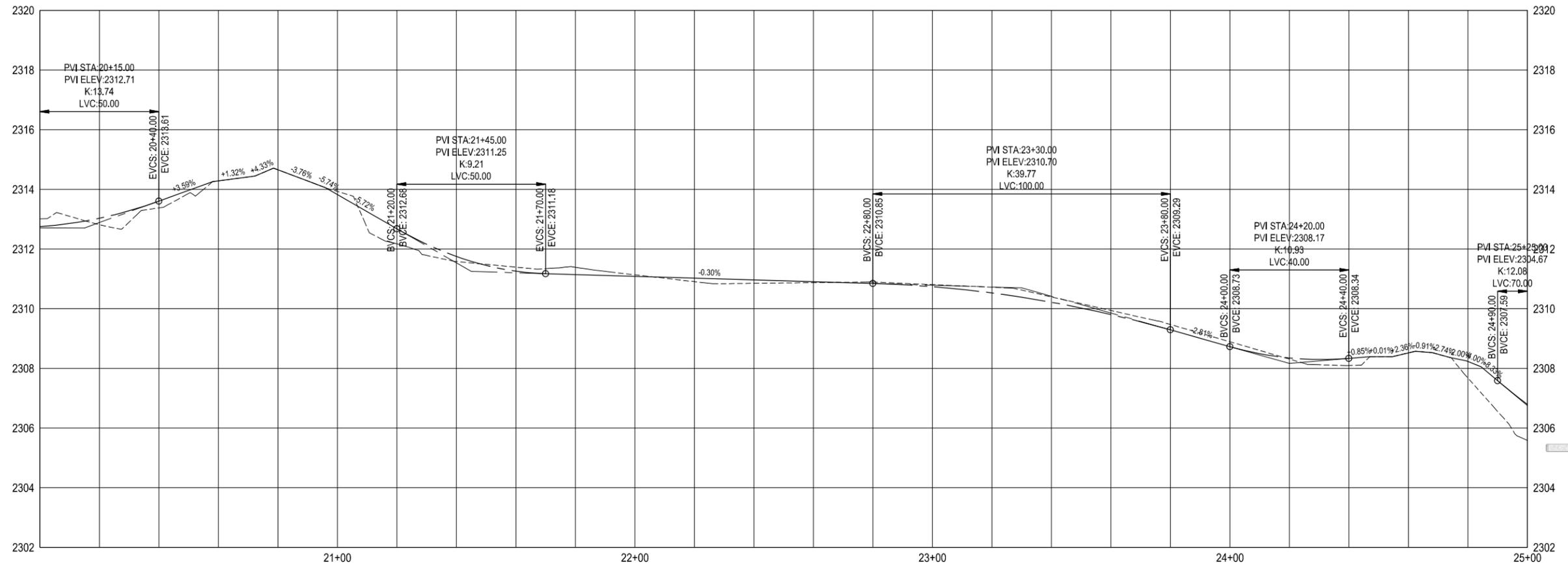
Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Moya\Desktop\S14-00-076\CAD\DESIGN\SHEETS\C-4.dwg 10/28/2014 2:41:15 PM



NOT APPROVED



Revision No.	Date	By	Description

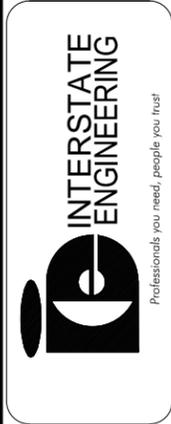
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

Project No.: S14-00-076
 Date: 10/14

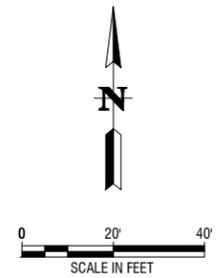
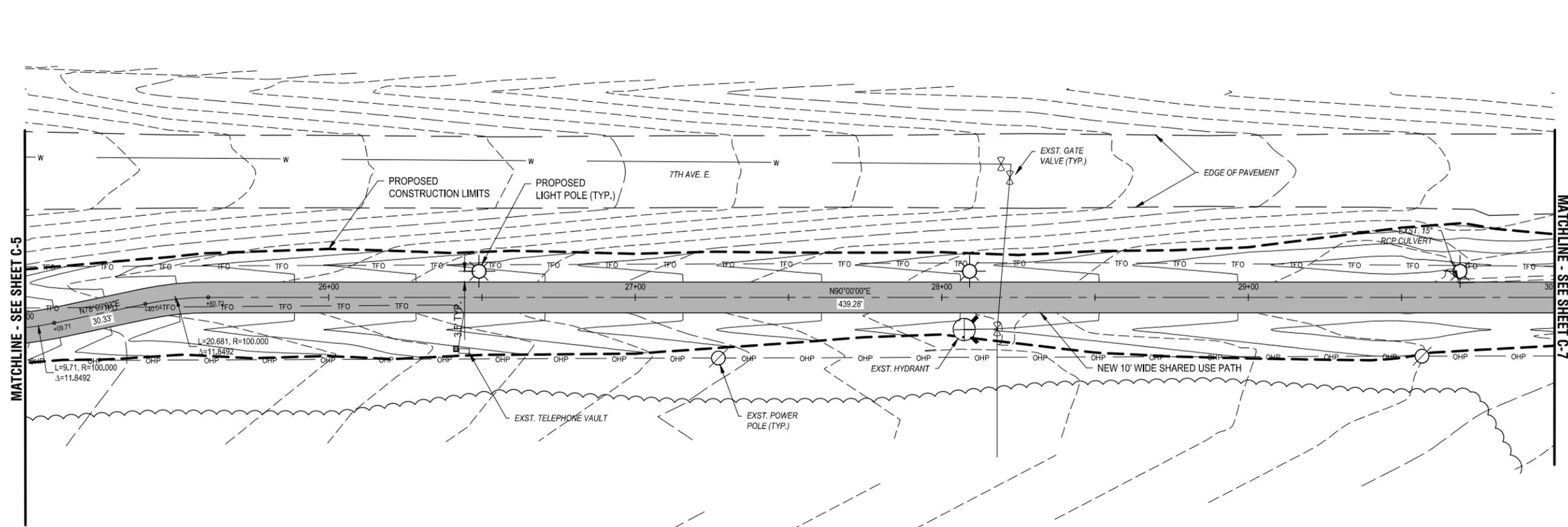
Drawn By: V.M.M.
 Checked By: C.C.C.

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

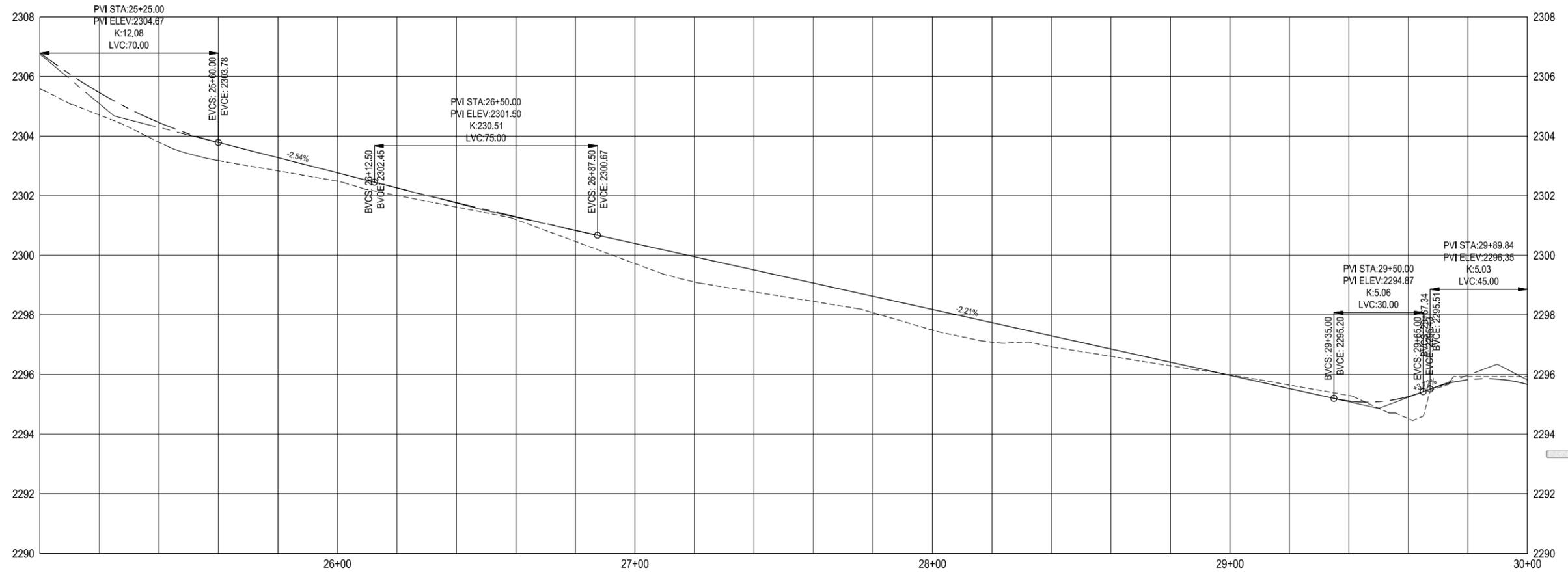
Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Moya\Desktop\S14-00-076\CAD\DESIGN\SHEETS\C-5.dwg 10/28/2014 2:41:47 PM



NOT APPROVED



Revision No.	Date	By	Description

7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

Drawn By: V.M.M.
 Checked By: C.C.C.

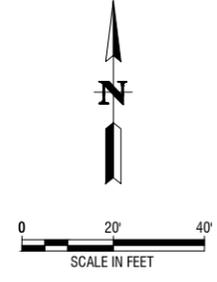
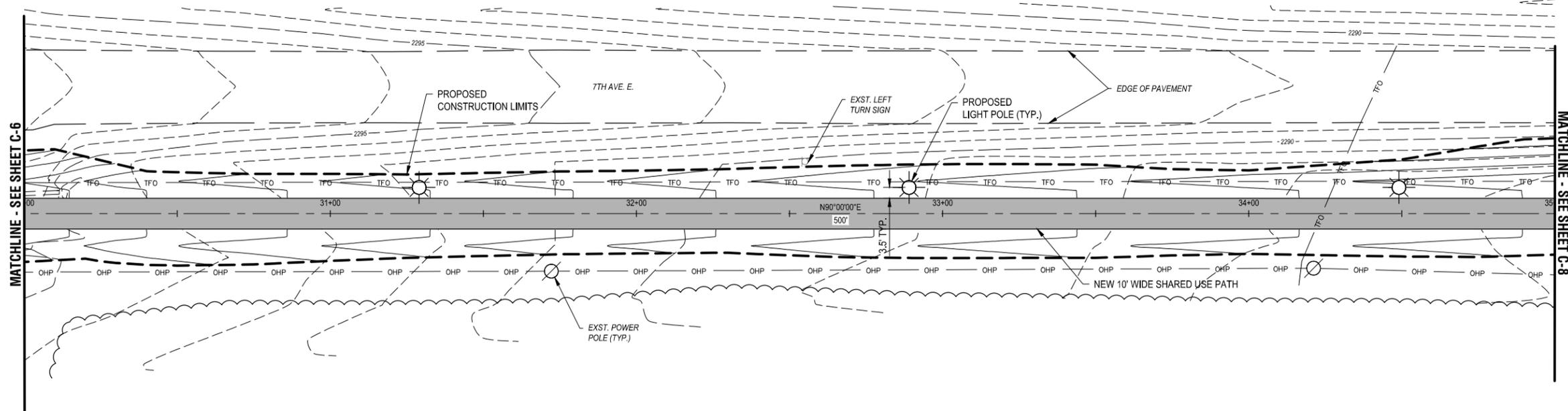
Project No.: S14-00-076
 Date: 10/14

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

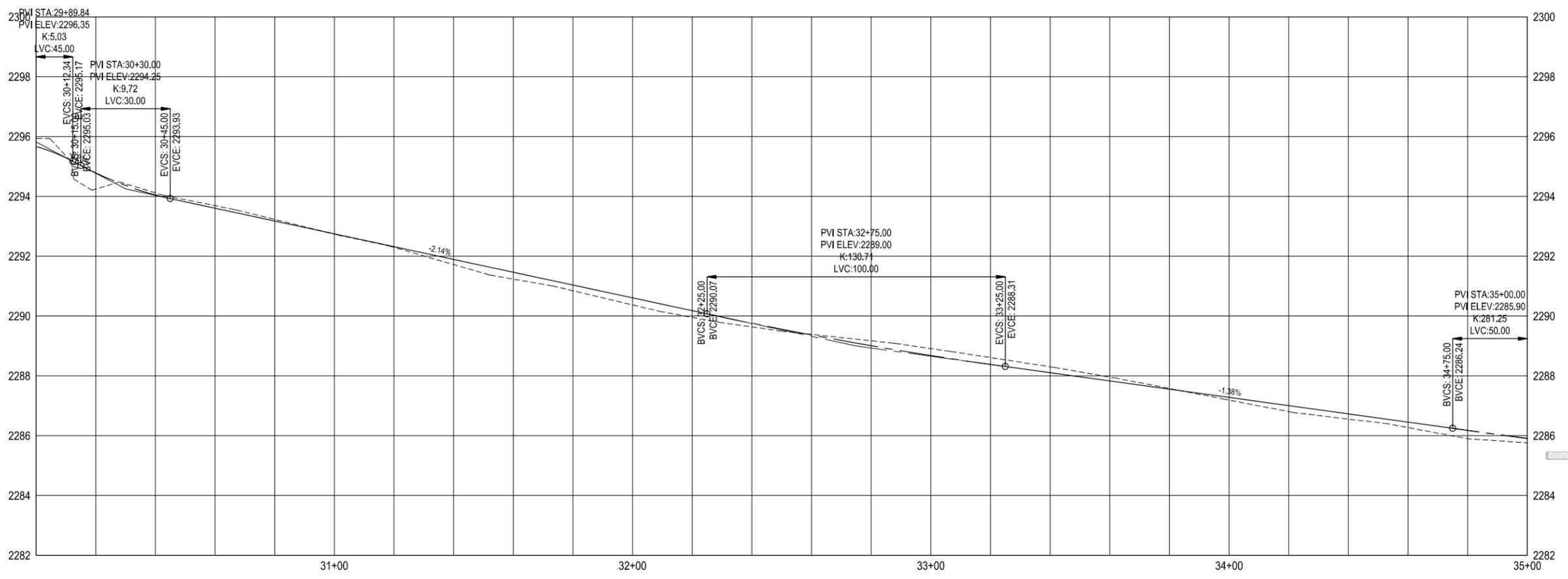
Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Moya\Desktop\S14-00-076\CAD\DESIGN\SHEETS\C-6.dwg 10/28/2014 2:42:15 PM



NOT APPROVED



Revision No.	Date	By	Description

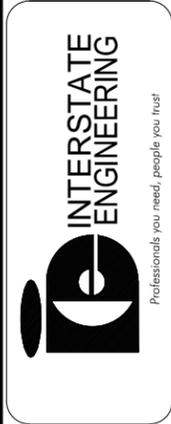
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

Drawn By: V.A.M.M.
 Checked By: C.C.C.

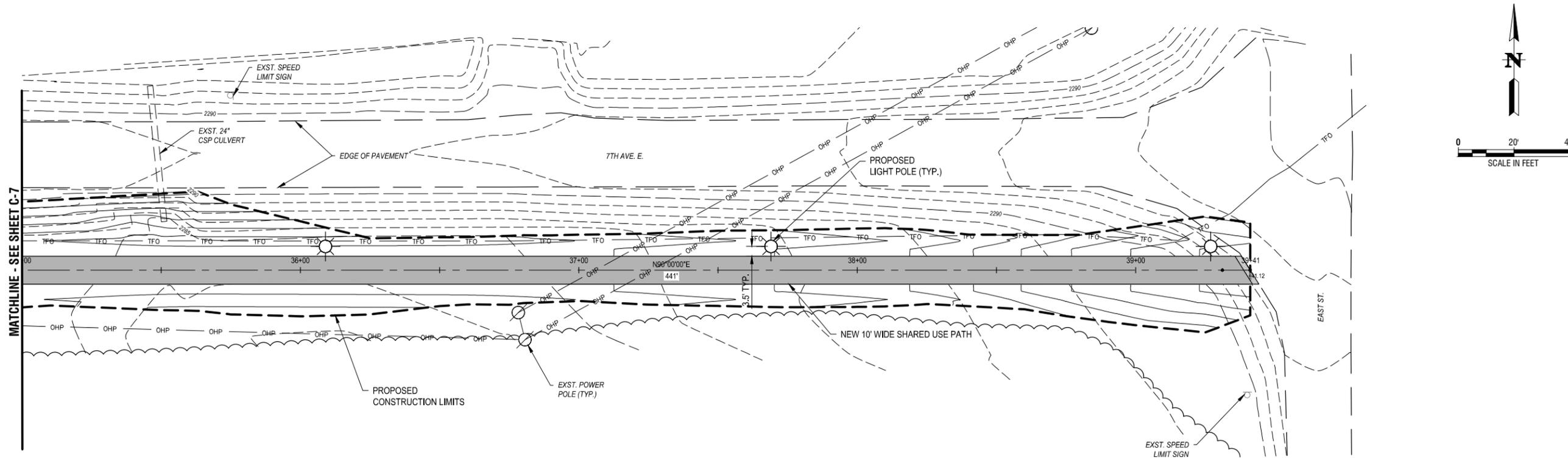
Project No.: S14-00-076
 Date: 10/14

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

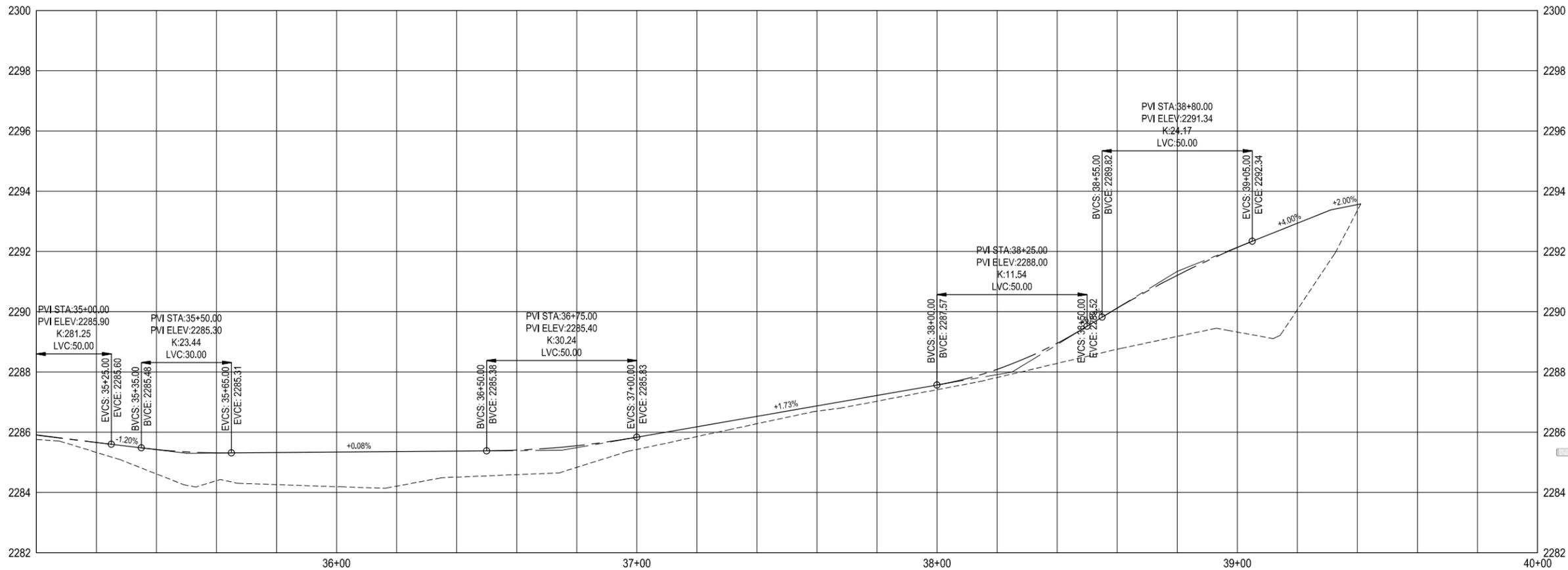
Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Moya\Desktop\S14-00-076\CAD\DESIGN\SHEETS\C-7.dwg 10/28/2014 2:42:45 PM



NOT APPROVED



Revision No.	Date	By	Description

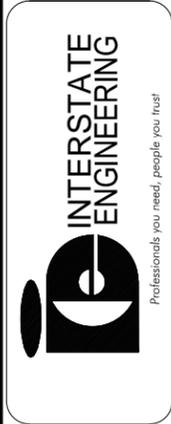
7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
SHARED USE PATH

Project No.: S14-00-076
 Date: 10/14

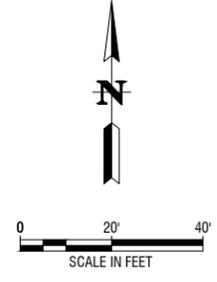
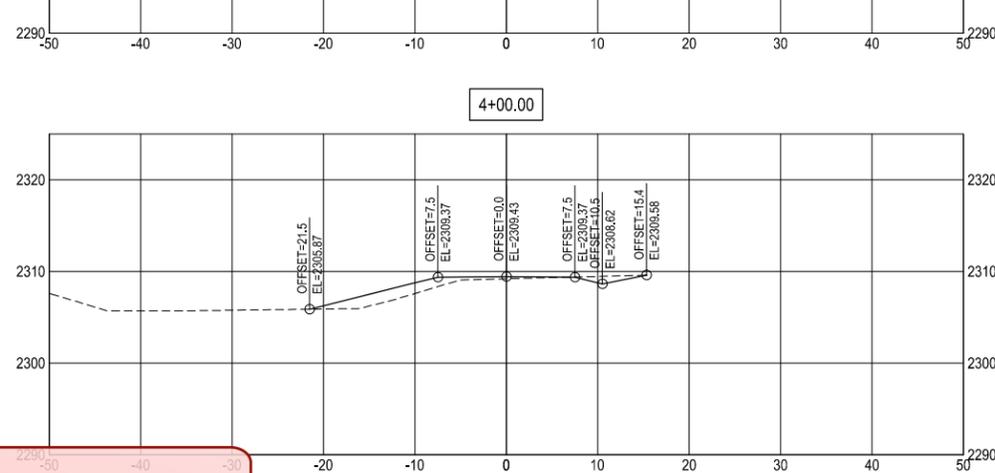
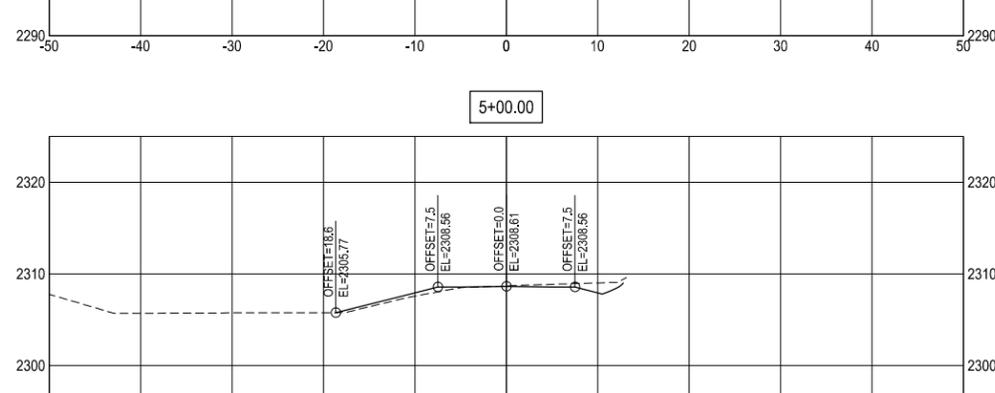
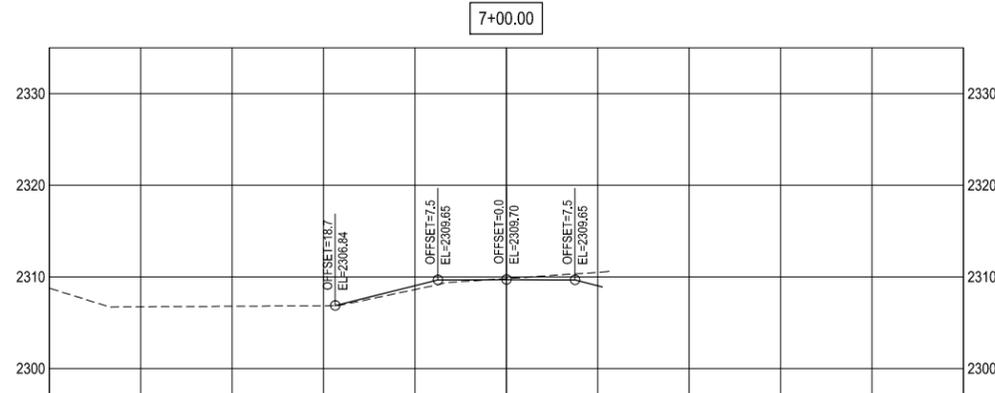
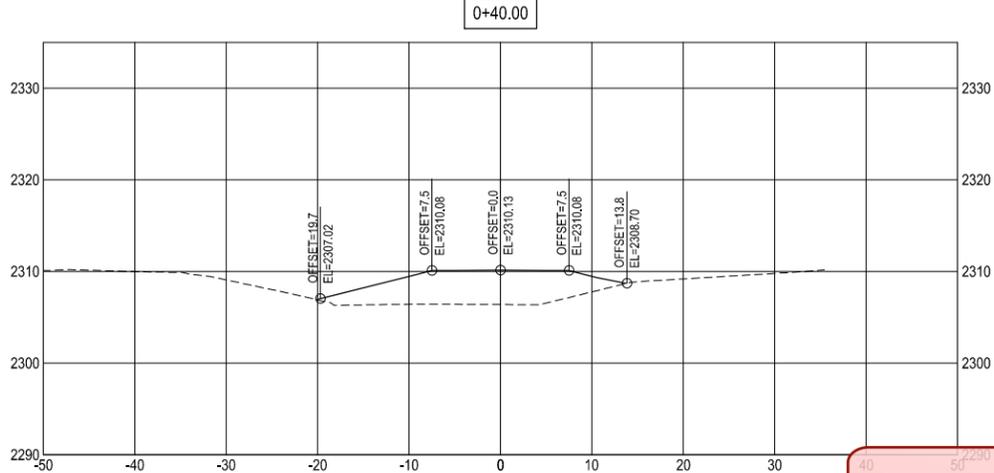
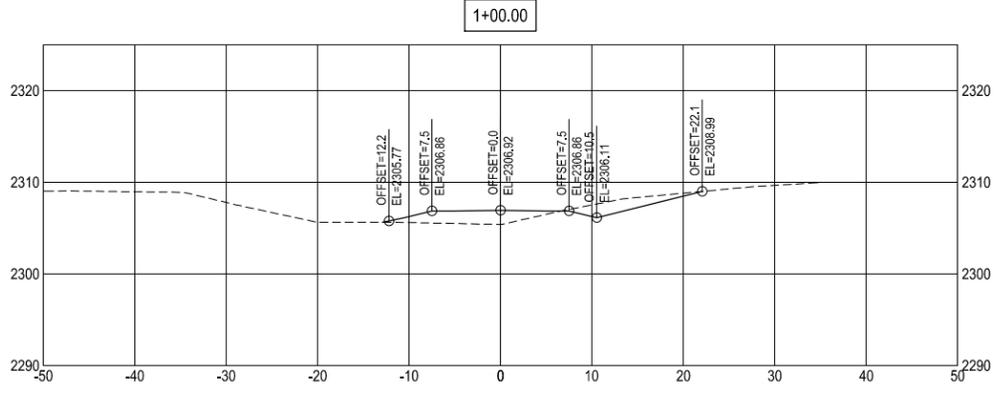
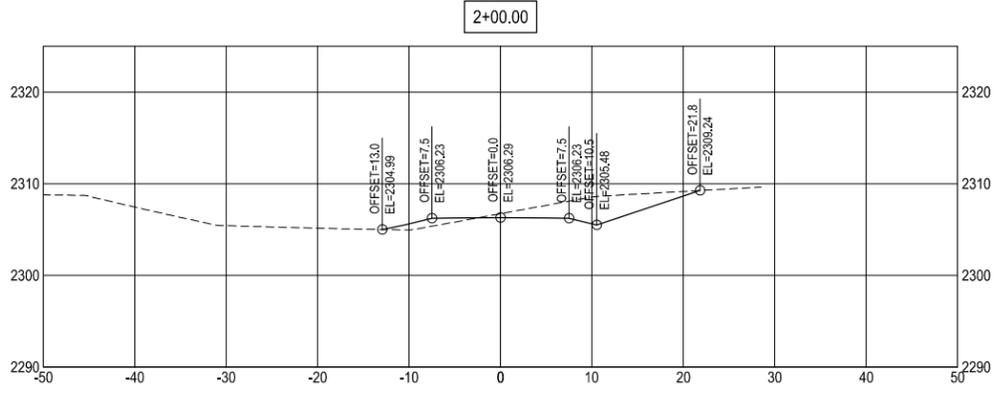
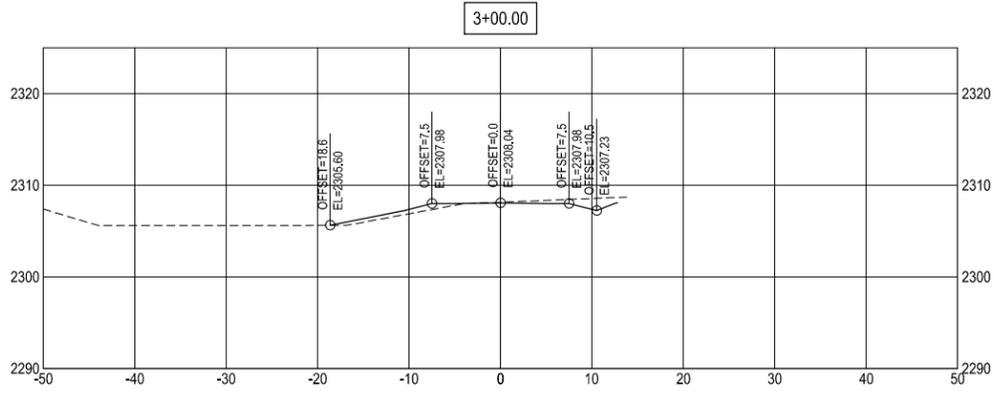
Drawn By: V.M.M.
 Checked By: C.C.C.

Interstate Engineering, Inc.
 P.O. Box 20953
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

Other offices in Minnesota, North Dakota and South Dakota



C:\Users\Veronica.Moya\Desktop\S14-00-076\CAD\DESIGN\SHEETS\C-8.dwg 10/28/2014 2:43:15 PM



NOT APPROVED

Revision No.	Date	By	Description

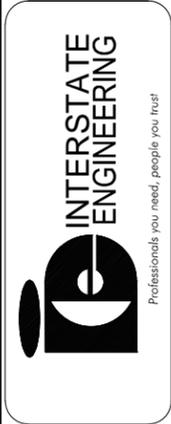
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

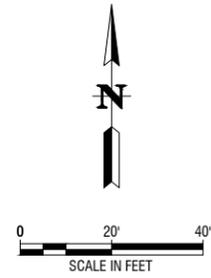
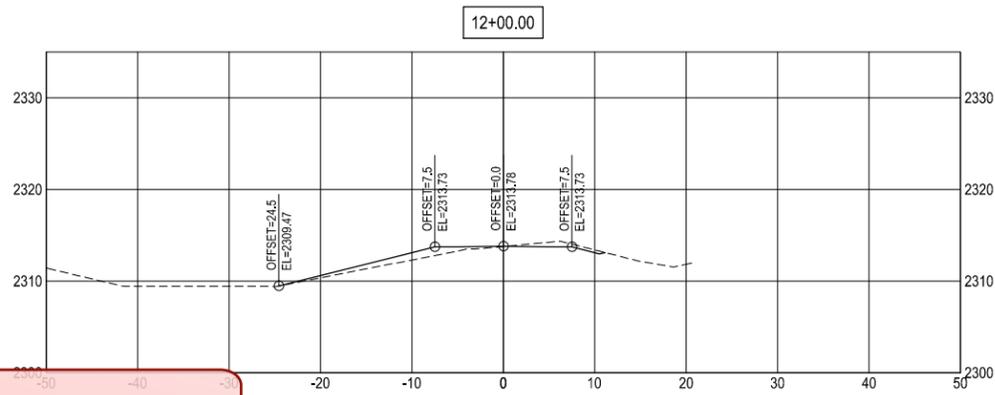
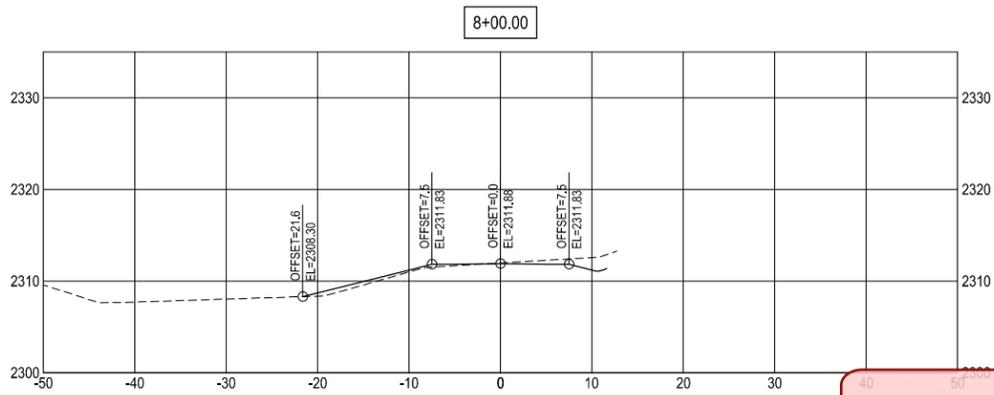
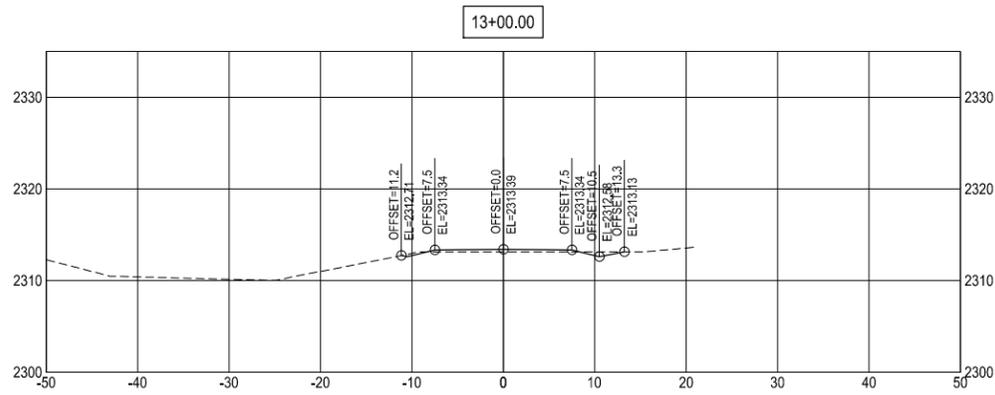
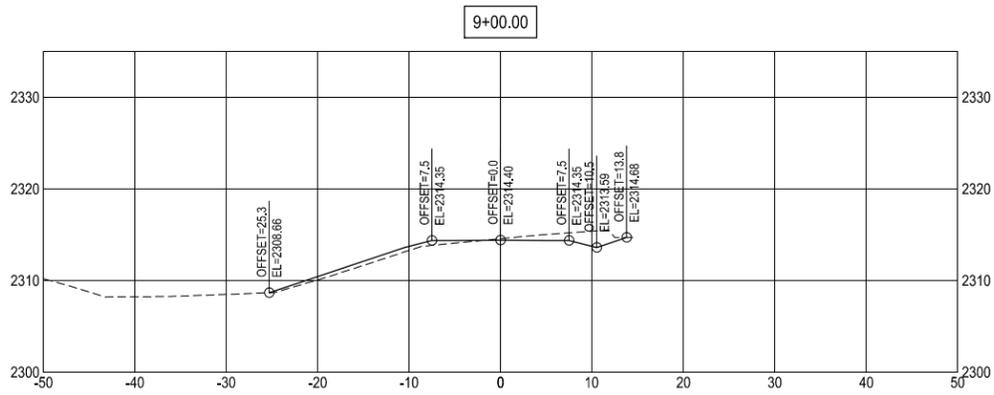
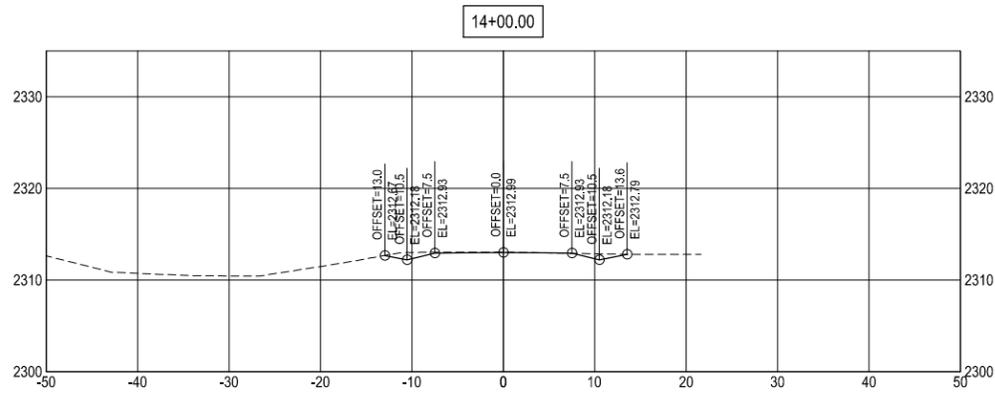
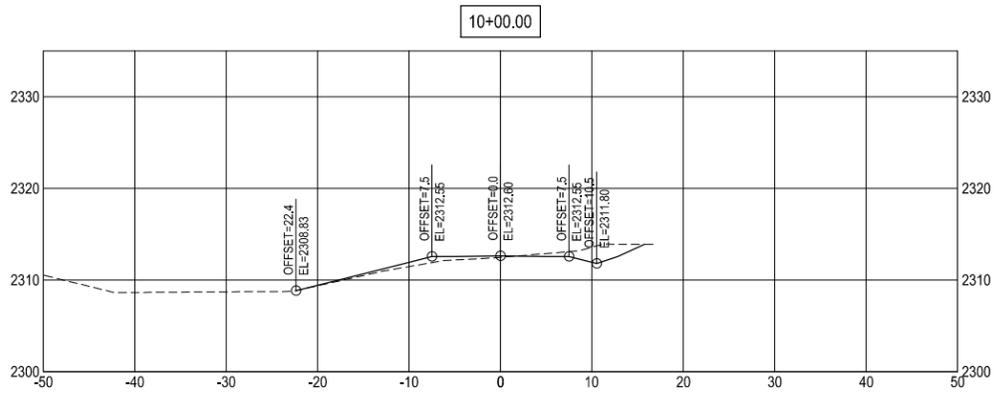
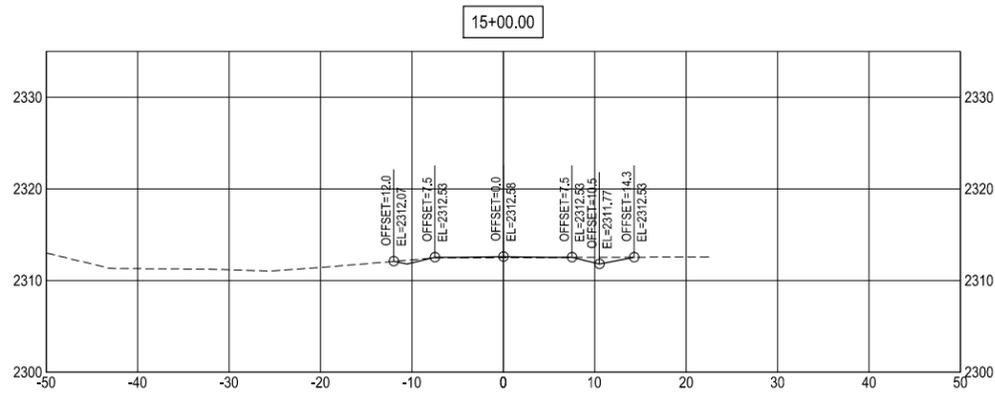
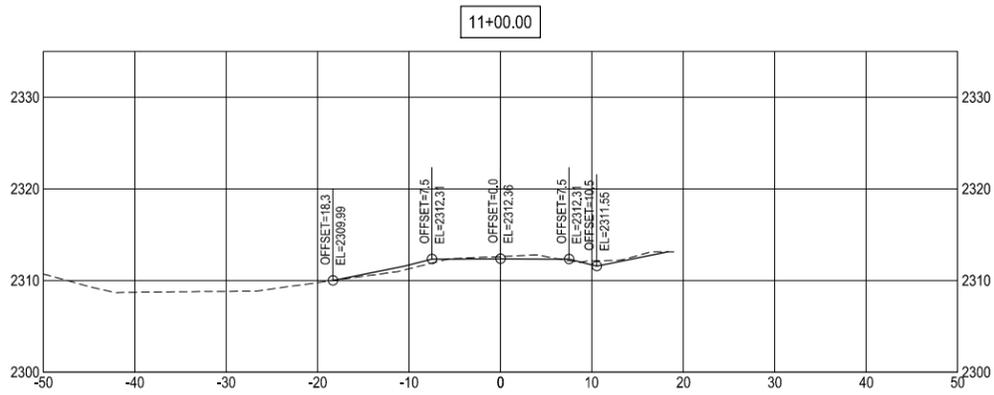
Project No.: S14-00-076
 Date: 10/14

Drawn By: V.M.M.
 Checked By: C.C.C.

Interstate Engineering, Inc.
 P.O. Box 20953
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

Other offices in Minnesota, North Dakota and South Dakota





NOT APPROVED

Revision No.	Date	By	Description

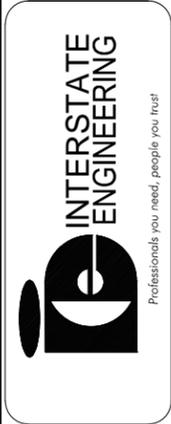
**7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
SHARED USE PATH**

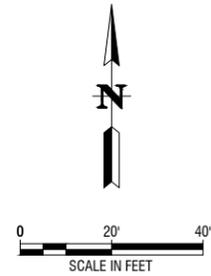
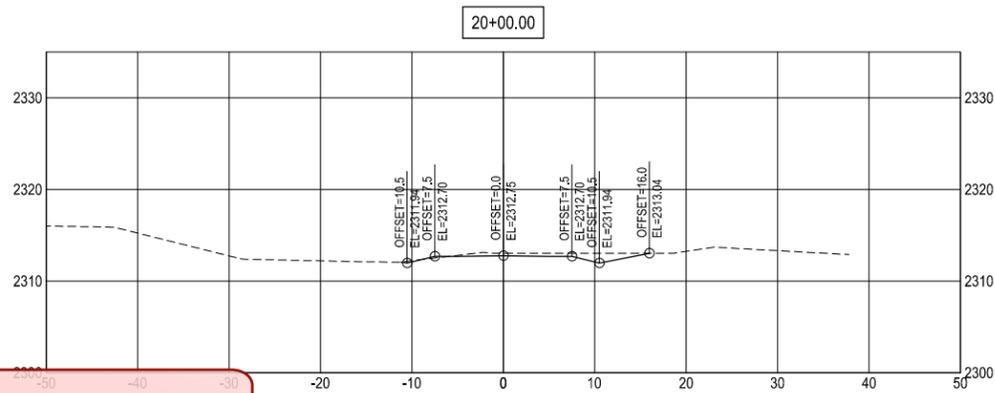
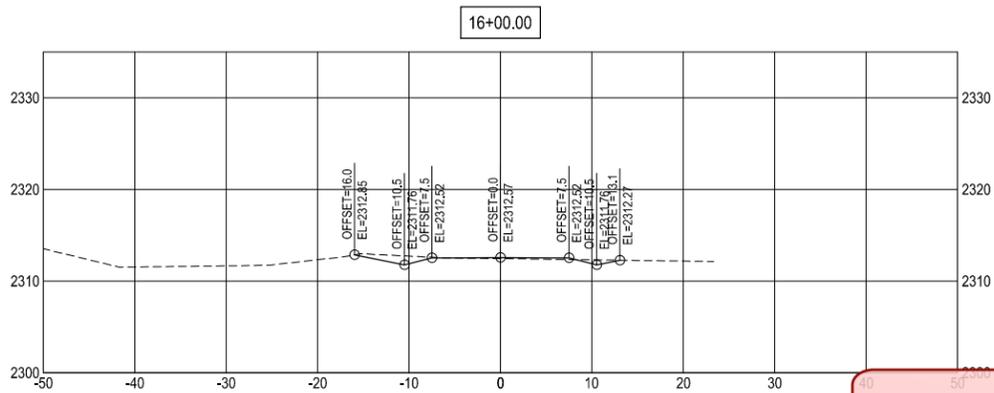
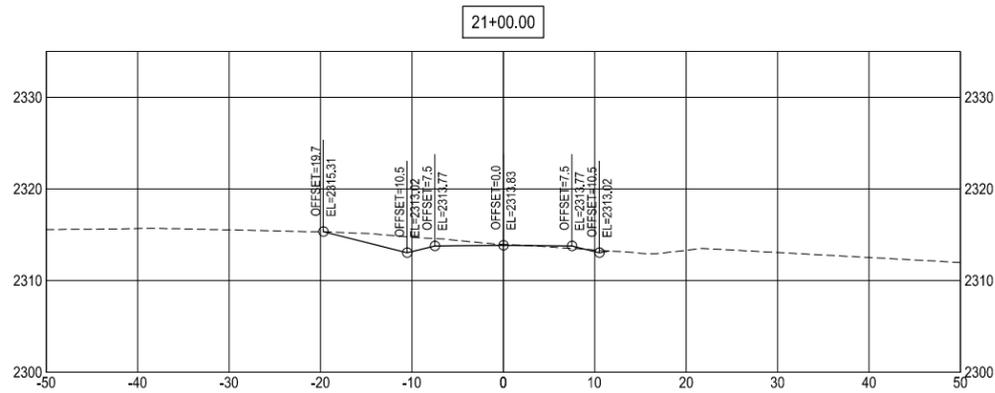
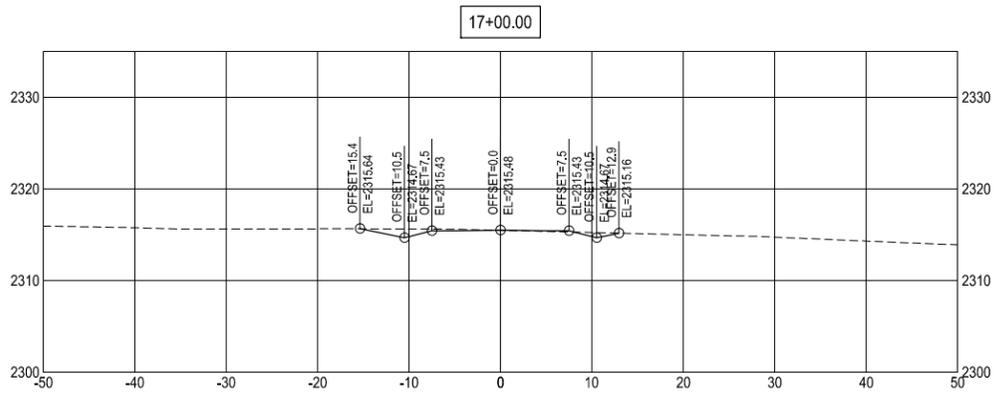
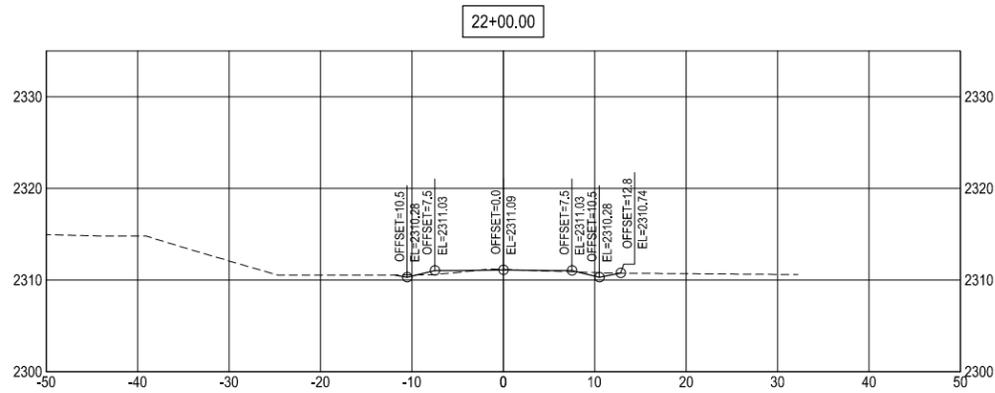
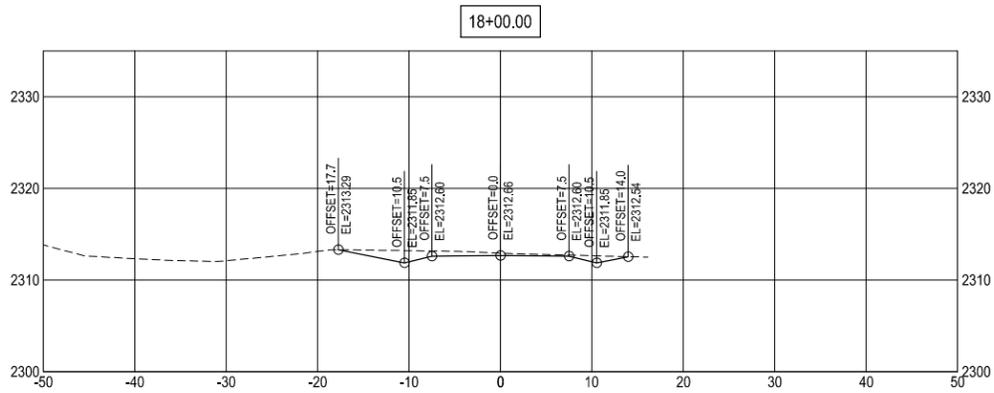
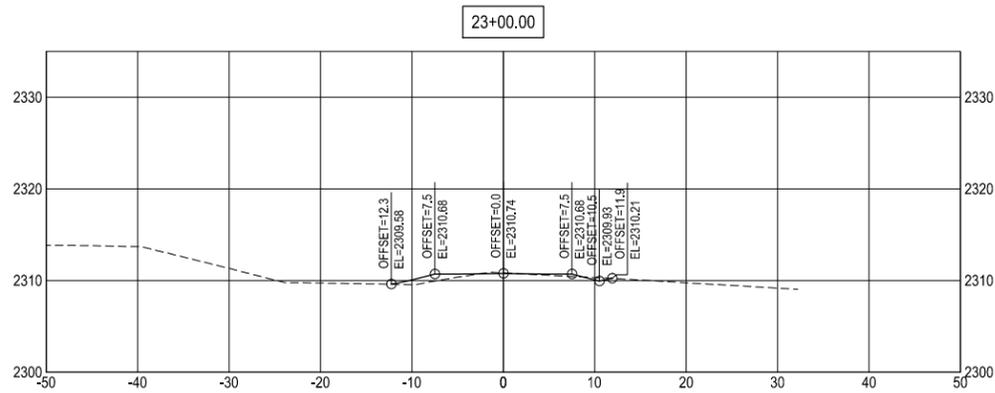
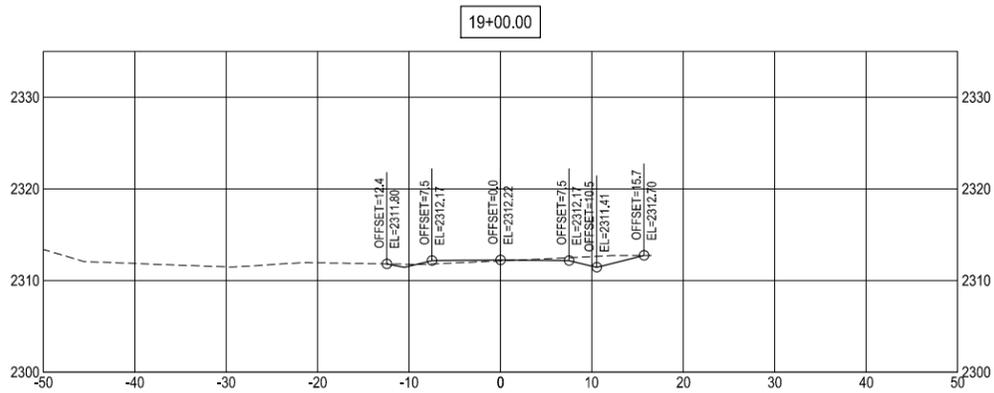
Project No.: S14-00-076
Date: 10/14

Drawn By: V.M.M.
Checked By: C.C.C.

Interstate Engineering, Inc.
P.O. Box 20953
1211 Grand Avenue Suite 6
Billings, Montana 59104
Ph (406) 256-1920
Fax (406) 256-9178
www.intersteeng.com

Other offices in Minnesota, North Dakota and South Dakota





Revision No.	Date	By	Description

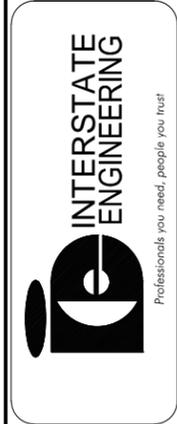
7TH AVENUE SHARED USE PATH
 RAY, NORTH DAKOTA
 SHARED USE PATH

Project No.: S14-00-076
 Date: 10/14

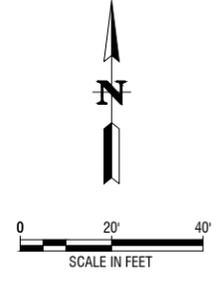
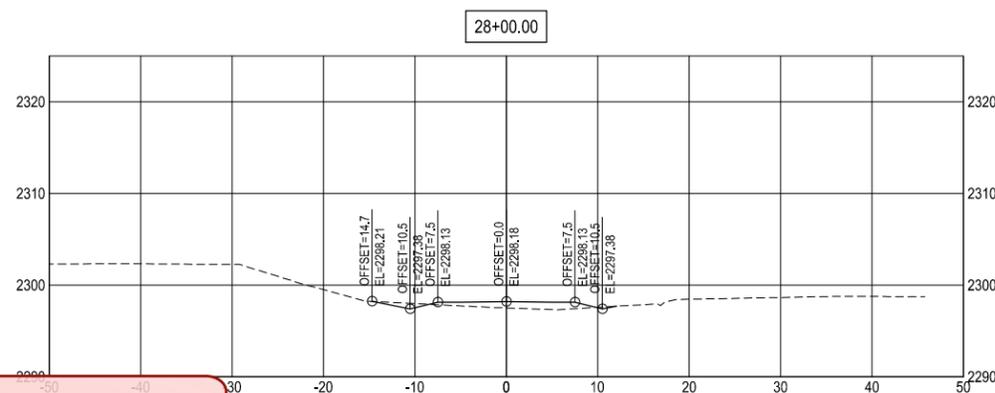
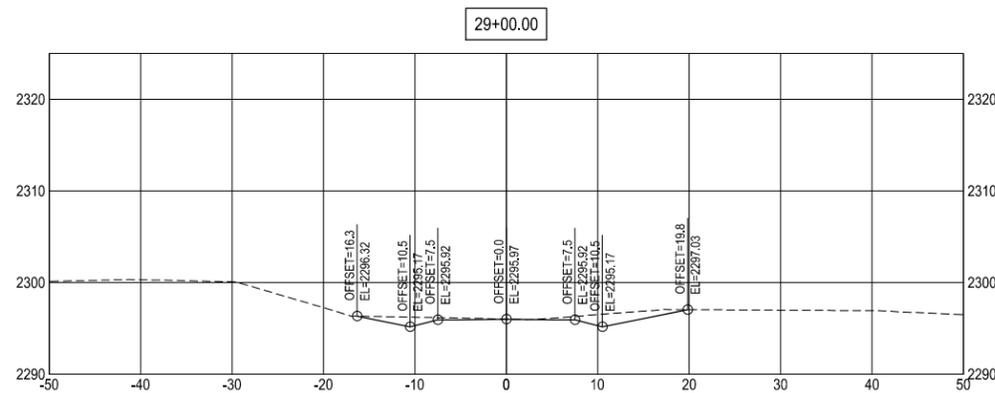
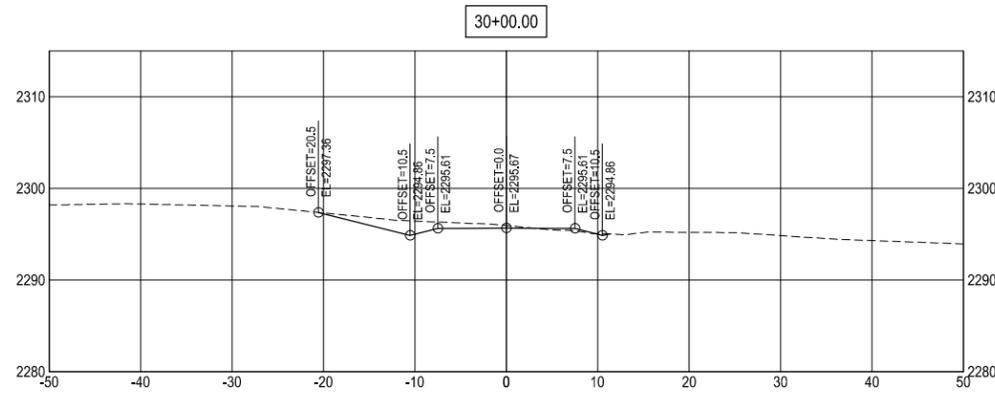
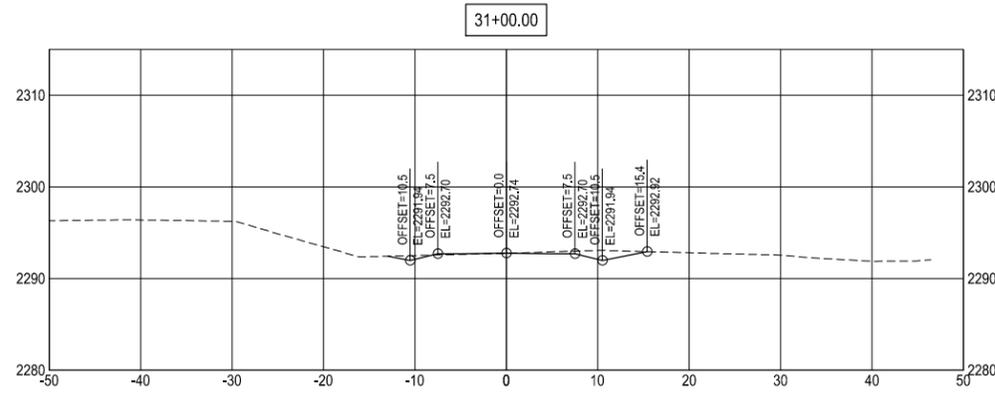
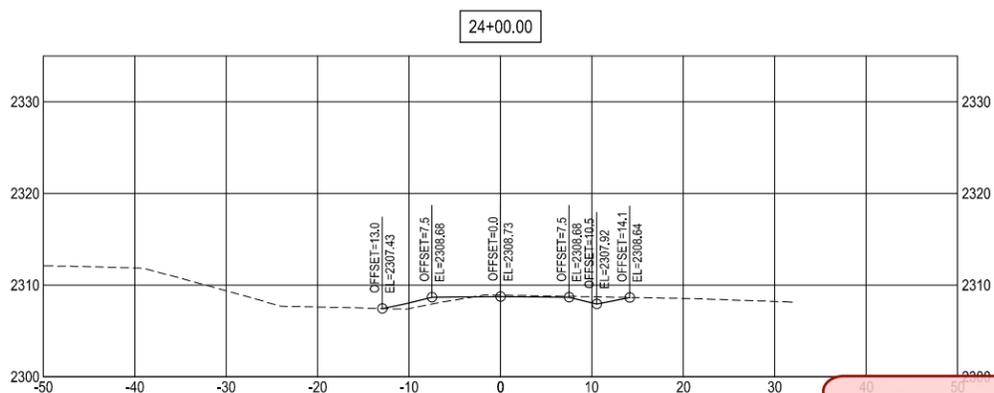
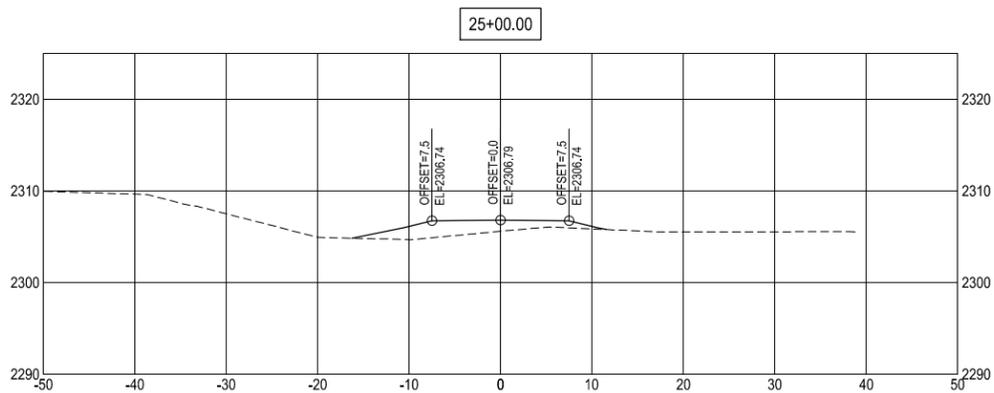
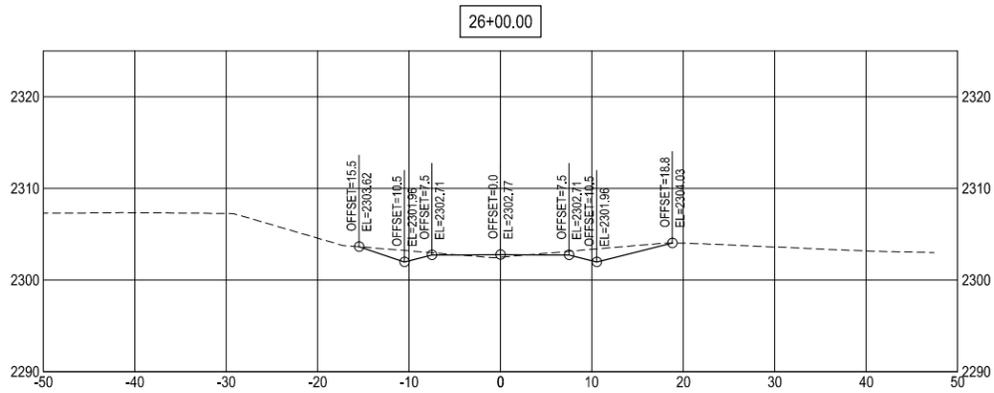
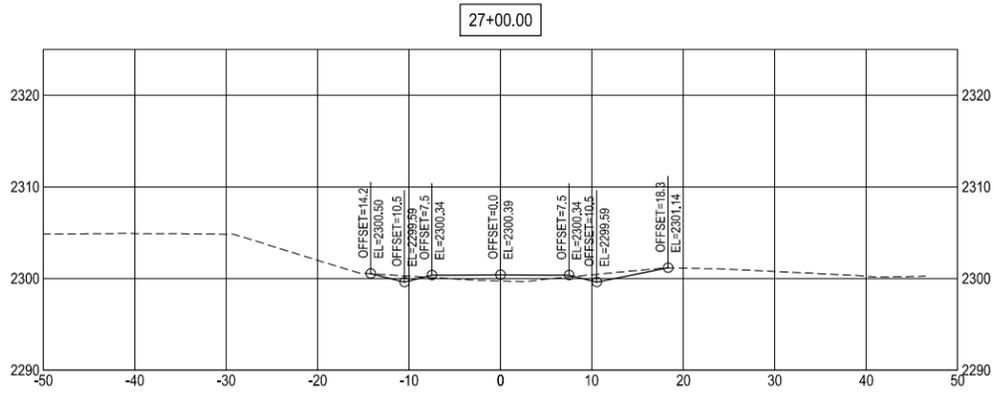
Drawn By: V.M.M.
 Checked By: C.C.C.

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.intersteeng.com

Other offices in Minnesota, North Dakota and South Dakota



NOT APPROVED



NOT APPROVED

Revision No.	Date	By	Description

7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
SHARED USE PATH

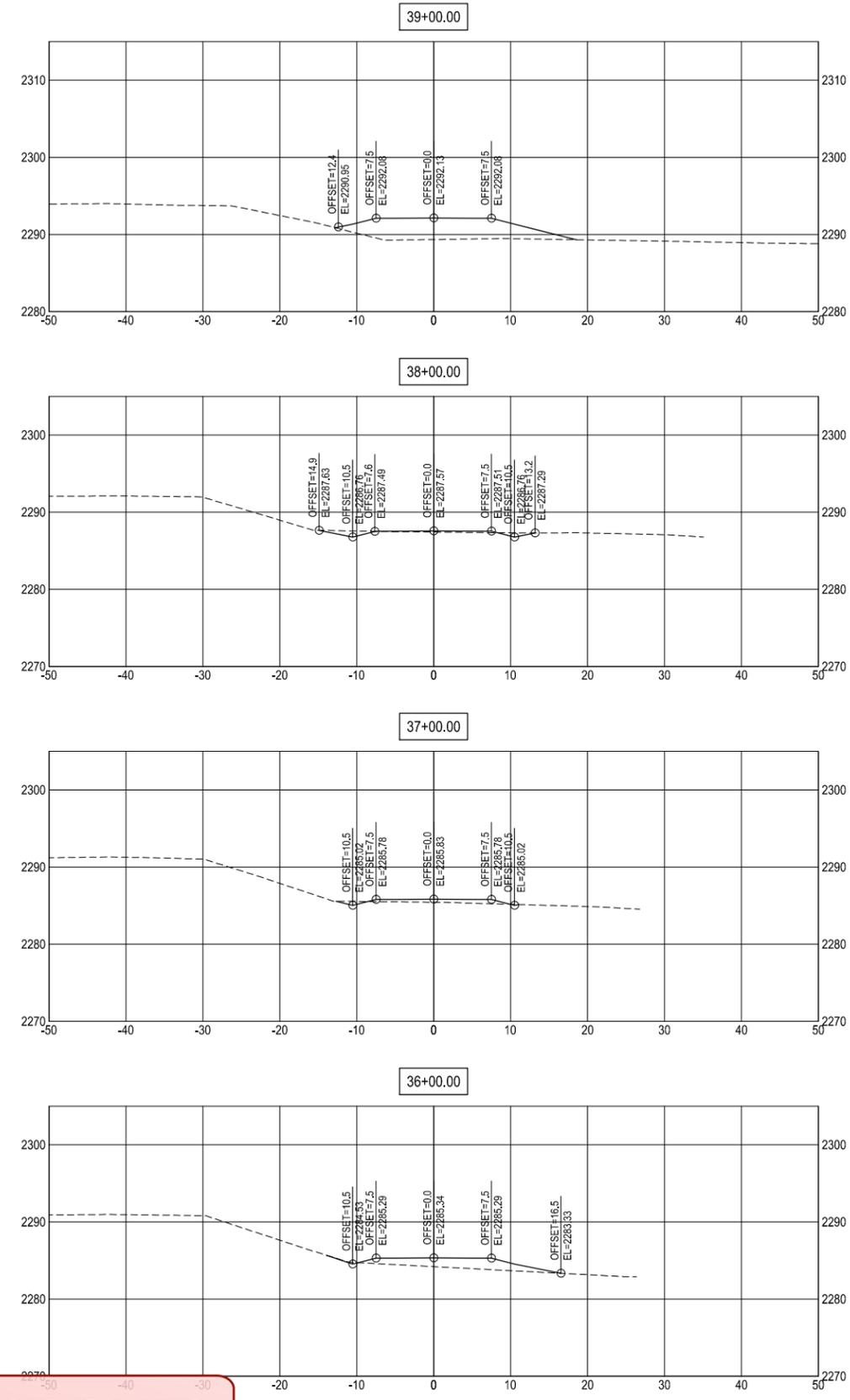
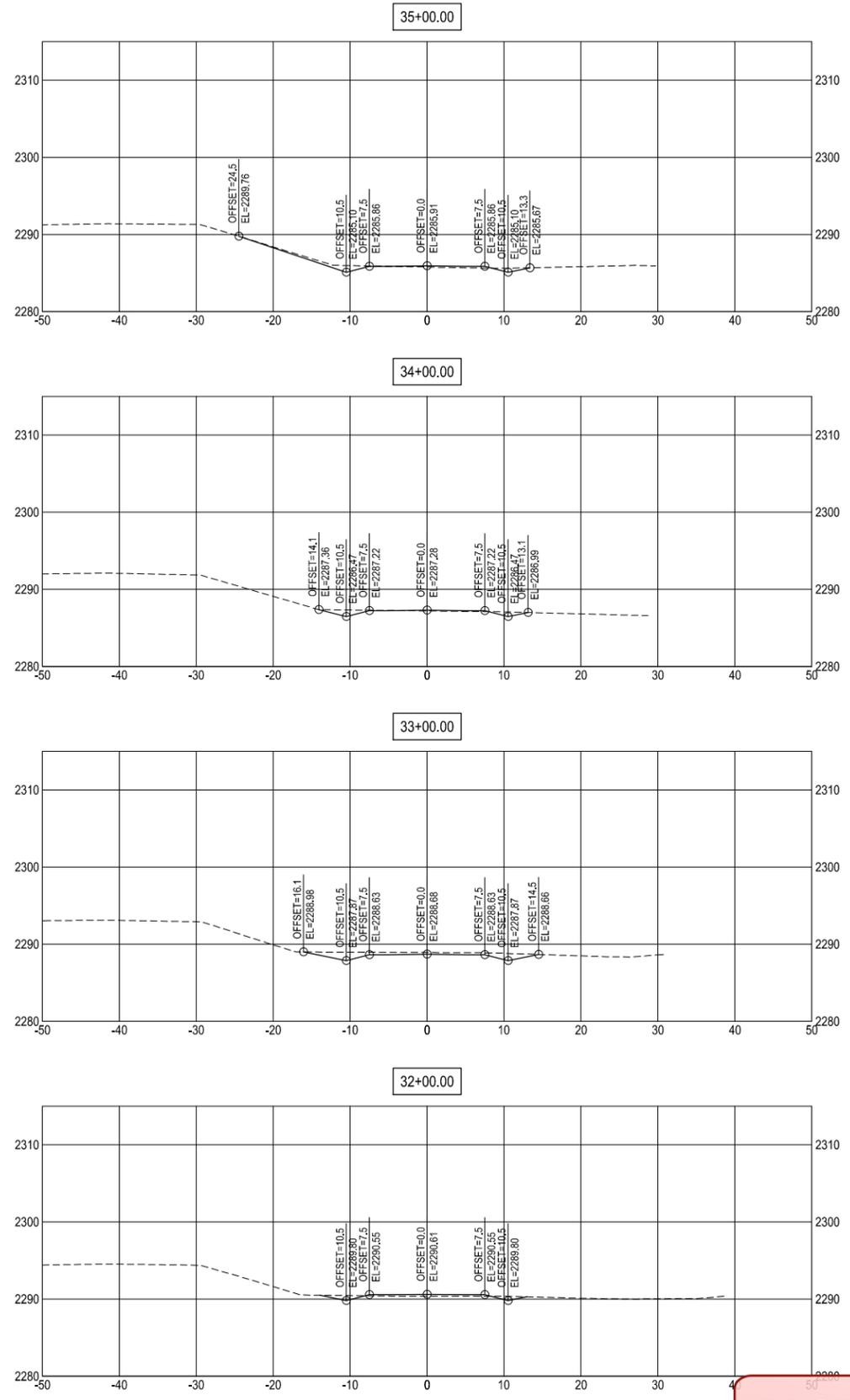
Project No.: S14-00-076
 Date: 10/14

Drawn By: V.M.M.
 Checked By: C.C.C.

Interstate Engineering, Inc.
 P.O. Box 20953
 1211 Grand Avenue Suite 6
 Billings, Montana 59104
 Ph (406) 256-1920
 Fax (406) 256-9178
 www.interstateeng.com

Other offices in Minnesota, North Dakota and South Dakota





NOT APPROVED

C:\Users\Veronica.Meyer\Desktop\S14-00-076\CAD\DESIGN\SHEETS\SECC-01.dwg 10/28/2014 2:52:26 PM

Revision No.	Date	By	Description

**7TH AVENUE SHARED USE PATH
RAY, NORTH DAKOTA
SHARED USE PATH**

Project No.: S14-00-076
Date: 10/14

Drawn By: V.M.M.
Checked By: C.C.C.

Interstate Engineering, Inc.
P.O. Box 20953
1211 Grand Avenue Suite 6
Billings, Montana 59104
Ph (406) 256-1920
Fax (406) 256-9178
www.intersteeng.com
Other offices in Minnesota, North Dakota and South Dakota



SAFE ROUTES TO SCHOOL– PHASE II

Project No.

PCN

TAC-0053(026)

20772

RAY, NORTH DAKOTA

Prepared by

**CITY OF RAY, NORTH DAKOTA
RAY, NORTH DAKOTA
City Commission President
Ken Munson**

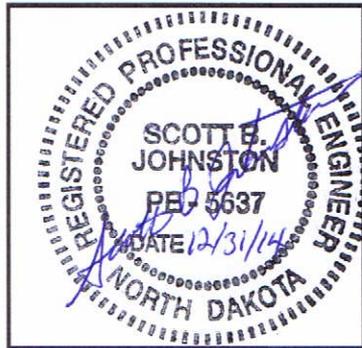
Principal Author: Interstate Engineering, Inc.

RAY SRTS – PHASE II

CITY OF RAY, NORTH DAKOTA

CERTIFICATION

I hereby certify that this report was prepared by me or under my direct supervision and that I am a duly registered professional engineer under the laws of the State of North Dakota. This document was originally issued and sealed by Scott B. Johnston, Registration number PE-5637 on 12/31/14 and the original document is stored at the Williston office of Interstate Engineering, Inc.





Scott B. Johnston, P.E.

12/31/14

Date

EXECUTIVE SUMMARY

Table of Contents:

- A. Project Description**
 - B. Project Schedule**
 - C. Purpose of Project**
 - D. Need for Project**
 - E. Scope of Work**
 - F. Alternatives**
 - G. Public Concerns / Need for Public Input**
 - H. Programmatic Categorical Exclusion Category**
 - I. Comments from Draft Decision Document**
 - J. Executive Decisions**
- Attachments:** **ECL**
 SOV Responses
 Section 7 Determination Table
 Wetland Delineation Report

A. Project Description

Highway: 4th Avenue, 6th Avenue, Score Street, Comford Street
District: Williston Projects: TAC-0053(026)
From: City of Ray

Project TAC-0053(026)

B. Project Schedule

<u>Project</u>	<u>Plans Complete</u>	<u>Bid Opening</u>
TAC-0053(026)	01/30/2015	04/10//2015

C. Purpose of Project

This project is proposed to improve the City of Ray's existing sidewalk system. The project will provide sidewalks with ADA compliant ramps for accessibility to both students and pedestrians and replace deteriorated sidewalks. The project will provide a walking pathway for pedestrian and students and alleviate the need to meander on the existing roadway.

D. Need for Project

Existing Conditions: A majority of the project area consists primarily of grassed lawns with no definitive walking area within the existing right-of-way. The current sidewalk system has gaps and areas of no or partial sidewalk completion. Other areas consist of dilapidated concrete approaches, existing narrow sidewalks that terminate at alley approached or mid-block, and sporadic existing non-compliant accessibility ramps with no pathway continuity.

Deficiencies: The sidewalk system in this project area lacks continuity for pedestrian travel in the vicinity of the City of Ray School. The abrupt termination of existing sidewalk also creates conditions that force pedestrians and students to change travel from sidewalks to traveling on the street where sidewalks are not present. The existing concrete sidewalk is not ADA compliant and is deteriorated.

E. Scope of Work

The project will consist of constructing 5 ft or 6 ft wide sidewalks, ADA ramps and crossings with 6" wide crosswalk striping along various streets in the north central part of Ray near the City park and ball fields, north and east of the school.

Driveway approaches will be modified as necessary to match grades of the proper cross slope of the new sidewalk. Minimal grading will be done around driveways to match. No complete driveway replacement will be necessary.

The boulevard sidewalks will consist of 4 inches of concrete with 4 inches of aggregate base as shown in Figure 1.

The proposed project area is shown in Figure 2.
 *note 1%min to 2%max cross slope

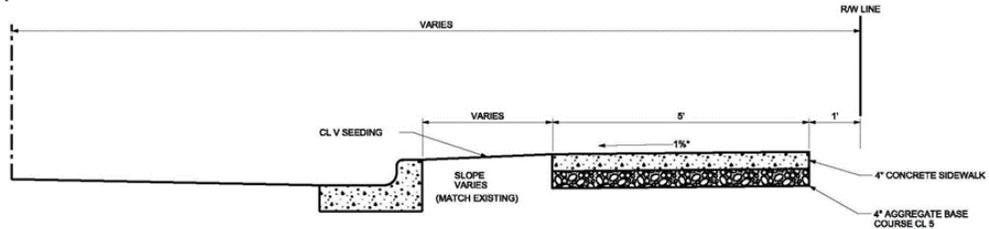


Figure 1



Figure 2

ENGINEER'S ESTIMATE OF PROBABLE PROJECT COSTS
 2015 Transportation Alternatives Program- Ray Safe Routes to School -Phase 2
 RAY, ND
 12/30/14

By: Interstate Engineering, Inc.
 SBJ
 IEI Project # S14-00-127

Spec	Code	Description	Quantity	Units	Unit Price	Extended Price
103	100	Contract Bond	1	LS	\$ 4,000.00	\$ 4,000.00
202	112	Removal of Concrete	300	SY	\$ 28.00	\$ 8,400.00
202	119	Saw Concrete	200	LF	\$ 6.00	\$ 1,200.00
202	130	Remove Curb & Gutter	400	LF	\$ 5.00	\$ 2,000.00
302	120	Aggregate Base Course CL 5	500	Ton	\$ 30.00	\$ 15,000.00
702	100	Mobilization	1	LS	\$20,000.00	\$ 20,000.00
704	100	Flagging	10	MHR	\$ 20.00	\$ 200.00
704	1000	Traffic Control Signs	600	UNIT	\$ 3.00	\$ 1,800.00
704	1052	Type III Barricades	12	EA	\$ 100.00	\$ 1,200.00
704	1065	Traffic Cones	40	EA	\$ 5.00	\$ 200.00
708	1400	Weighted Fiber Rolls	20	LF	\$ 5.00	\$ 100.00
708	2900	Seeding-Hydro Mulch	500	SY	\$ 10.00	\$ 5,000.00
722	6240	Adjust Utility Appurtanance	2	EA	\$ 150.00	\$ 300.00
748	140	Curb & Gutter - Type I	400	LF	\$ 40.00	\$ 16,000.00
750	115	Sidewalk Concrete 4IN	1660	SY	\$ 70.00	\$ 116,200.00
750	1016	Driveway Concrete 6IN Reinforced	510	SY	\$ 110.00	\$ 56,100.00
750	2115	Detectable Warning Panels	260	SF	\$ 35.00	\$ 9,100.00
754	117	Flat Sheet for Signs - Type 3A Refl Sheeting	60	SF	\$ 25.00	\$ 1,500.00
754	206	Steel Galv Posts- Telescoping Perforated Tube	140	LF	\$ 25.00	\$ 3,500.00
754	592	Reset Sign Panel	4	EA	\$ 200.00	\$ 800.00
754	593	Reset Sign Support	4	EA	\$ 200.00	\$ 800.00
762	1106	Pvmt Mk Painted 6 In Line	600	LF	\$ 6.00	\$ 3,600.00

Preliminary Opinion of Probable Construction Cost = \$ 267,000.00
 Estimated Engineering Fees = \$ 70,000.00
 Preliminary Opinion of Probable Project Cost= **\$ 337,000.00**

Funding:
 2015 Federal Funding Application= \$200,000 TAP Funding = \$200,000.00
 City Responsibility = \$ 137,000.00
\$337,000.00

F. Alternatives

Alternative A: No Build

Alternative B: This alternative will add new sidewalk, ADA ramps and crossings along Score Street from 3rd Avenue, north to 7th Avenue completing a route from the school to the north side of town. The project will also add new sidewalk, ADA ramps and crossings to complete a route on Fourth Avenue from Score Street to existing sidewalk near High Street. New sidewalk, ADA ramps and crossings will be added to link to the area of town by the ball fields on Comford Street from Fourth Avenue to Sixth Avenue and on 6th Avenue from Score Street to Comford Street. Driveway approaches will be modified as needed to match grades. Crosswalk striping will be added at various locations.

Estimated Construction Costs - \$267,000

G. Public Concerns / Need for Public Input:

The project was discussed at the City Commission meetings on November 25, 2013 and November 10th, 2014 at the Ray City Hall, Ray, ND.

H. How does this project fit into the Programmatic Categorical Exclusion category?

The proposed project falls into the "Miscellaneous Items: Pedestrian Facilities/Curb Ramps and Pavement Marking" category for a Programmatic Categorical Exclusion in accordance with Section II-02.02.01 of the design manual. The required documents for the Programmatic Categorical Exclusion are this decision document, Worksheets A, B, and C which are included in the ECL.

I. Comments from the Draft Decision Document

Local Government Division:

1. Cover Sheet – a. Title Missing.
 - *This change has been made.*
 - b. SRTS- Spell out.
 - *This change has been made.*
2. Page 1 – Include a table of contents. Highway: should list the city streets affected by the project. Delete "Urban project"- Ray is not defined as "urban" city.
 - *These changes have been made.*
3. Page 2 – a. Is there any concern for drainage with a 1% cross slope on the sidewalk?
 - *A note of "1% min to 2% max" was added.*
 - b. Where new curb ramps and crosswalks are being installed, what do they connect to on the other side?
 - *Curb ramps and crosswalks will connect to new or existing ramps.*
 - c. Can we get verification that the City Park and Ball Fields will not be impacted permanently or temporarily by the project? The Section 4(f) review will need to be further evaluated if any impacts are anticipated.

-The Ray Recreation Area identified in the SOV response from ND Parks and Recreation is over 1 block away from the project area and will not be impacted. This was confirmed over the phone.

4. Page 3 – The Contract Bond unit price seems high; please reference the NDDOT Design Manual for Contract Bond recommendations (Section III-21).

- This has been updated using recommendations.

5. Page 4 – a. Delete Environmental Issues section- This is taken care of in the attached checklist (Worksheet A).

- This section has been deleted.

b. This section (H.) needs to define which portions of the Programmatic CATEX apply to this project; i.e. sidewalks and striping.

- These changes have been made.

6. Page 5 – Formatting of signature line.

- This change has been made.

7. Programmatic Categorical Exclusion- a. Delete description from Project # line.

- This change has been made.

b. Add title for signature block on last line.

- This change has been made.

8. Worksheet A – a. Delete description from Project # line.

- This change has been made.

b. Have any residents raised concerns or expressed opposition to this project at city commission meetings?

-No residents have raised concerns or opposed this project.

9. Worksheet B - . Delete description from Project # line.

- This change has been made.

10. SOV Response – Please include any further coordination and state whether utility conflicts exist.

-At this time, no utility conflicts exist.

11. Wetland Delineation Report – Did Local Government verify the no wetland determination? Typically NWI is not sufficient as a sole source for an off-site wetland determination.

- No Local Government verification. However, the wetland in this area is out of the range of this project. Previous project in this area and onsite visit by designer confirmed the no wetland determination.

Attachments:

ECL

PROGRAMMATIC CATEGORICAL EXCLUSION

Project #: TAC-0053(026)

PCN: 20772

Date: 12/31/14

The work for this project is generally confined to the roadway surface with the exception of some Structural, Guardrail, ITS, and Miscellaneous types of work. Select all work items that will be applied to the project. The pavement marking will be replaced if removed or covered as a part of the project. The abbreviation in the parentheses designates the investment strategy in the Design Guidelines (PM = Preventive Maintenance, MiR = Minor Rehabilitation, SI = Structural Improvement, MaR = Major Rehabilitation, N/R = New/Reconstruction).

- The "Decision Document for ECL" is required for any work item selected below that is designated as: (MiR), (SI), (MaR), (N/R), or (-Dec.Doc.) and is also required for any projects located within the 12 major cities.
- Attach Worksheet C – Supporting Documentation for any work item selected below designated with (+)

1) Surface Treatments: Use additional pages to discuss the applicable Americans with Disabilities Act (ADA) of 1990 requirements. An asterisk (*) denotes ADA requirements may be required as a part of the project.

- | | |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <input type="checkbox"/> Seal Coat (PM)
<input type="checkbox"/> Slurry Seal (PM)
<input type="checkbox"/> Microsurfacing (PM)
<input type="checkbox"/> Macrosurfacing (PM)
<input type="checkbox"/> Crack Pouring/Sealing (PM)
<input type="checkbox"/> Route and Seal (PM)
<input type="checkbox"/> Repair of depressed cracks (PM)
<input type="checkbox"/> Dowel Bar Retrofit (PM)
<input type="checkbox"/> Mudjacking (PM)
<input type="checkbox"/> Grinding (PM)
<input type="checkbox"/> CPR ≤ 10% area per mile (PM) | <input type="checkbox"/> Aggregate Surfacing (PM)
<input type="checkbox"/> *Patching (PM)
<input type="checkbox"/> *HBP ≤ 2" (PM)
<input type="checkbox"/> *Mill and Overlay ≤ 2" (PM)
<input type="checkbox"/> *HBP up to 3" (MiR)
<input type="checkbox"/> *Mill and Overlay up to 3" (MiR)
<input type="checkbox"/> *HBP > 3" (SI)
<input type="checkbox"/> *White Topping (SI)
<input type="checkbox"/> *Cold in Place Recycle (MiR) |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

2) Minor Structural Work:

- | | |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <input type="checkbox"/> Mudjacking (PM)
<input type="checkbox"/> Approach Slabs (PM)
<input type="checkbox"/> Sealing Deck/Substructures (PM)
<input type="checkbox"/> Painting (PM) | <input type="checkbox"/> +Deck Replacement (N/R)
<input type="checkbox"/> +Deck Rehabilitation (MaR)
<input type="checkbox"/> +Abutment Repair (PM)
<input type="checkbox"/> +Slope Repair/ Protection (PM)
<input type="checkbox"/> +Rail Retrofit (PM) |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

3) Guardrail Work:

- Guardrail Removal, Resetting, or Installation (+if embankment required)

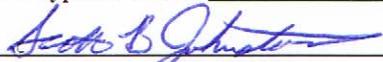
4) Intelligent Transportation Systems (ITS): The purpose is to install, replace, or update items.

- +Automated Traffic Recorder, ATR (-Dec.Doc.)
 +Weigh in Motion, WIM (-Dec.Doc.)
 +Environmental Sensor Station (ESS) (-Dec.Doc.)
 +NDDOT Road Weather Information System, RWIS (-Dec.Doc.)
 +Dynamic Message Signs, DMS (-Dec.Doc.)
 +Surveillance Camera (-Dec.Doc.)
 +Video Detection System for Signals

5) Miscellaneous Items: The purpose is to install, replace, or update items.

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <input type="checkbox"/> Rumble Strips (PM)
<input checked="" type="checkbox"/> Pavement Markings (PM)
<input type="checkbox"/> Railroad Warning Devices
<input type="checkbox"/> Fencing (-Dec.Doc.)
<input type="checkbox"/> +Traffic Signals (-Dec.Doc.)
<input type="checkbox"/> +Signs (-Dec.Doc.)
<input type="checkbox"/> +Lighting (-Dec.Doc.) | <input type="checkbox"/> +Flashing Beacon (-Dec.Doc.)
<input checked="" type="checkbox"/> +Pedestrian Facilities/Curb Ramps
<input type="checkbox"/> +Landscaping (-Dec.Doc.)
<input type="checkbox"/> +Pipe Repair/Replacement
<input type="checkbox"/> +Advanced Acquisition of ROW (-Dec.Doc.)
<input type="checkbox"/> +Disposal of Excess ROW
<input type="checkbox"/> +Environmental Mitigation (-Dec.Doc.) |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

6) Other Types of Work:



 Scott B Johnston, Interstate Engineering, Inc.

12/31/14

 Date

 print name, NDDOT District/Division

 Date

Worksheet A

ENVIRONMENTAL CHECK LIST

Project #: TAC-0053 (026)

PCN: 20772

Date: 12/31/14

Answer the environmental questions below. If any of the questions below are answered "YES", attach Worksheet C – Supporting Documentation and coordinate with the ETS Division for environmental and cultural clearance or permits.

Yes No SECTION 4(F)/6(F): Are there any impacts to Section 4(f) or 6(f) property (public owned land of a public park, recreation area, or wildlife and waterfowl refuge and historic sites)?

Yes No HISTORIC / ARCHAEOLOGICAL: Are there any impacts to HISTORIC / ARCHAEOLOGICAL properties?

Yes No THREATENED OR ENDANGERED SPECIES: Does the action affect species or critical habitat protected by the Endangered Species Act?

Yes No RIGHT OF WAY: Is there any action required for new right of way or temporary easement, minor access change, relocations, and does it have any risk of hazardous materials involvement?

Yes No PRIME AND UNIQUE FARMLAND: Does this action involve the acquisition of prime and unique farmland?

Yes No STATE SCENIC RIVER: Does the action impact the Little Missouri River, a state designated scenic river?

Yes No FLOODPLAIN/FLOODWAY: Based on SOV Letter #6 to the ND State Water Commission, is the project located in a floodplain or floodway? (SOVs are required for projects as noted in section II-05.02.01 of the Design Manual).

Yes No WETLANDS: Does the action have temporary or permanent impacts to any wetlands?

Yes No NOISE: Is this action a Type I action in accordance with 23 CFR 772 and does it significantly impact noise levels? (Increases the carrying capacity of the roadway by allowing more volume).

Yes No AIR: Will the action significantly impact air quality?

Yes No WATER QUALITY: Does the project disturb greater than one acre and if it does a Pollution Discharge Elimination System NDPDES (NDDOH) / NPDES (EPA) Construction Permit must be obtained.

Yes No PUBLIC CONTROVERSY: Is this action controversial?



Scott B. Johnston – Interstate Engineering, Inc.

12/31/14
Date

**Worksheet B
PROJECT DATA**

Project #: TAC-0053 (026)

PCN: 20772

Date: 12/31/14

Estimated Cost: \$ 267,000

STIP Cost: \$ _____

Traffic Volumes: Current ADT: _____ Percent Trucks: _____

Highway Classification

Cross Section

- | | | | |
|------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------|-----------------------------------------------------|---------------------------------------------|
| <input type="checkbox"/> Interstate | <input type="checkbox"/> State Corridor | <input checked="" type="checkbox"/> Rural Two Lane | <input type="checkbox"/> Interstate Divided |
| <input type="checkbox"/> Interregional | <input checked="" type="checkbox"/> District Corridor | <input type="checkbox"/> Rural Divided | |
| <input type="checkbox"/> District Collector | | <input type="checkbox"/> Urban (all cross sections) | |
| <input type="checkbox"/> Other - | | | |

Type of Project: Installation and Construction of New 5-Foot Sidewalk in Existing Right-of-Way

Existing Roadway Width: 40' Rdwy (80' ROW) *Proposed Roadway Width: N/A Minimum Roadway Width Required: N/A

****If the proposed roadway width is less than the minimum roadway width required in the Design Guidelines, forward to the Planning & Programming Division***

Any Design Exception required for this project: Yes _____ No X
(Check the Design Guidelines)

Have any safety issues been identified through the Statewide Safety Program? Yes _____ No X

If yes, what: _____

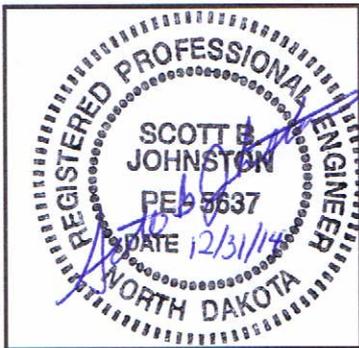
What are the ADA Requirements to be considered for improvements?
Construction of Sidewalks will be in Compliance with "Guide to the 2010 ADA Standards for Accessible Design".

Have any traffic control devices been identified that do not comply with the current MUTCD?
Yes _____ No X If yes, what? _____

Do all railroad crossings have adequate protective devices in place? Yes NA No NA
If no, what? Project is not located within a railroad corridor, as such the question is "Not Applicable".

Is the project located within 5 miles of an airport? *Yes _____ No X
****If yes, send general SOV letter and coordinate with NDDOT Utilities Engineer***

Cost Effective Analysis:



This document was originally issued and sealed by Scott B. Johnston, Registration number PE-5637 on 12/31/2014 and the original document is stored at the North Dakota Department of Transportation.

Scott B. Johnston
Scott B. Johnston
12/31/14
Date

Worksheet C

SUPPORTING DOCUMENTATION

The following items will be attached to this worksheet to assist with the approval of environmental and cultural clearance through coordination with the ETS Division:

- Attach project location map with township, section, and range.
- Attach cross sections and plan and profile sheets from the old grading plans showing the locations of work, any widened areas, limits of construction and disturbance, any devices, location of the power and route to get to it, required trenching, controllers, and soil disturbance areas.
- Attach any available pictures of the proposed work areas.

The project has been reviewed based on the accuracy of the information provided.

Comments: _____

Environmental Services, ETS Division

Date

Comments: _____

Cultural Resource Services, ETS Division

Date

Attachment:

SOV RESPONSES

51304233



DEPARTMENT OF THE ARMY
CORPS OF ENGINEERS, OMAHA DISTRICT
NORTH DAKOTA REGULATORY OFFICE
1513 SOUTH 12TH STREET
BISMARCK ND 58504-6640

September 17, 2014

North Dakota Regulatory Office

[NWO-2014-2095-BIS]

Interstate Engineering, Inc.
Attn: Mr. Dean Peterson, PE
P.O. Box 2236
Williston, North Dakota 58802

Dear Mr. Peterson:

This is in reference to your letter dated September 10, 2014, on behalf of the City of Ray, concerning a proposed sidewalk, ADA ramps, and crossings along Score Street. The project is located Section 9, Township 156 North, Range 97 West, in Ray, North Dakota.

Based upon the information provided in your letter, we have determined that the project will not involve a discharge of dredge or fill material into waters of the United States; therefore, Department of the Army authorization under Section 404 of the Clean Water Act is not required.

Should the project area change or require the discharge of dredged or fill material into other waters of the United States, including wetlands, you should contact this office for a new permit determination. The fact that a Section 404 permit is not required does not eliminate the need to obtain other Federal, state, tribal, and local approvals that may have regulatory jurisdiction over this project.

The Omaha District, North Dakota Regulatory Office is committed to providing quality and timely service to our customers. In an effort to improve customer service, please take a moment to complete our Customer Service Survey found on our website at http://corpsmapu.usace.army.mil/cm_apex/f?p=regulatory_survey. If you do not have Internet access, you may call and request a paper copy of the survey that you can complete and return to us by mail or fax.

If you have any questions regarding this determination, jurisdiction, or future projects please feel free to contact Swade Hammond of this office at telephone number (701) 255-0015, Ext. 2012 and refer to Corps No. **NWO-2014-2095-BIS**.

LF	<input type="checkbox"/>	MH	<input type="checkbox"/>
JK	<input type="checkbox"/>	SJ	<input type="checkbox"/>
DL	<input type="checkbox"/>	DP	<input type="checkbox"/>
BT	<input type="checkbox"/>	RJ	<input type="checkbox"/>
	<input type="checkbox"/>		<input type="checkbox"/>
	<input type="checkbox"/>		<input type="checkbox"/>

RECEIVED
SEP 19 2014

Sincerely,

Daniel E. Cimarosti
Regulatory Program Manager
North Dakota

Enclosures

Scott Johnston

From: Dean Peterson
Sent: Tuesday, October 07, 2014 4:30 PM
To: Chris Clark
Subject: S1304233 - Ray TAP

Chris,

I forgot to mention another contact I had on this project. David Bell of MDU called me on 9-23-14 after getting my letter. He said that MDU has power poles and street lights in the area. I told him that at this time we don't have a detailed plan to check for specific conflicts, but we might be able to make adjustments to the sidewalk location to avoid the poles, and that we will continue to coordinate with him as the project progresses.

Dean Peterson, PE

Sr. Project Engineer
Interstate Engineering
202 13th Street W
Williston, ND 58801
Office: 701-774-3637
Cell: 701-320-4295
Fax: 701-774-3638

Dean.Peterson@interstateeng.com

Professionals you need, people you trust

513-04-233



Jack Dabrymple, Governor
Mark A. Zimmerman, Director

1600 East Century Avenue, Suite 3
Bismarck, ND 58503-0649
Phone 701-328-5357
Fax 701-328-5363
E-mail parkrec@nd.gov
www.parkrec.nd.gov

June 15, 2012

Mr. Dean Peterson
Interstate Engineering
202 13th Street W
PO Box 2236
Williston, ND 58802-2236

RECEIVED
SEP 22 2014

Re: TAC-0053(026),PCN 20772 Ray SRTS-Phases II Williams County

Dear Mr. Peterson,

The North Dakota Parks and Recreation Department (the Department) has reviewed the above referenced proposed roadway improvement within the City of Ray, North Dakota.

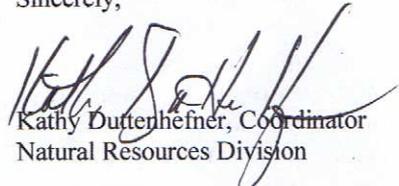
Our agency scope of authority and expertise covers recreation and biological resources (in particular rare plants and ecological communities). The project as defined does not affect state park lands that we manage but may affect state Land and Water Conservation Fund (LWCF) project sites that we manage. A map with LWCF project locations has been attached. All LWCF sites received assistance from the federal LWCF program and are under protection of section 6(f) of the LWCF Act. Any property taken from within the 6f boundary of these sites must be replaced with property of equal market value. Should any public or private utilities need to be added or relocated on the LWCF recreational lands, the NDPRD must be consulted prior to any action taken. Please contact Kevin Stankiewicz (701-328-5364 or kstankiewicz@nd.gov if additional LWCF information is needed.

The North Dakota Natural Heritage biological conservation database has been reviewed to determine if any plant or animal species of concern or other significant ecological communities are known to occur within an approximate one-mile radius of the project area. Based on this review, there are no documented occurrences in our database within or adjacent to project area. Because this information is not based on a comprehensive inventory, there may be species of concern or otherwise significant ecological communities in the area that are not represented in the database. The lack of data for any project area cannot be construed to mean that no significant features are present. The absence of data may indicate that the project area has not been surveyed, rather than confirm that the area lacks natural heritage resources.

The Department recommends that the project be accomplished with minimal impacts and that all efforts be made to ensure that critical habitats not be disturbed in the project area to help secure rare species conservation in North Dakota. Regarding any reclamation efforts, we recommend that any impacted areas be revegetated with species native to the project area.

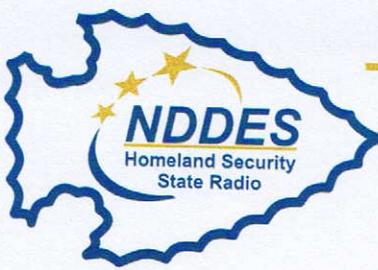
We appreciate your commitment to rare plant, animal and ecological community conservation, management and inter-agency cooperation to date. For additional information please contact me at (701-328-5370 or kgduttonhefner@nd.gov). Thank you for the opportunity to comment on this proposed project.

Sincerely,


Kathy Duttonhefner, Coordinator
Natural Resources Division

R.USNDNHI*2014-153KD9/19/2014DL10.10.2014

.....
Play in our backyard!



ND Department of Emergency Services

PO Box 5511

Tel: (701) 328-8100

Email: nddes@nd.gov

Bismarck, ND 58506-5511

Fax: (701) 328-8181

Website: www.nd.gov/des

Ensuring a safe and secure homeland for all North Dakotans

October 7, 2014

Interstate Engineering, Inc.
Attn: Dean Peterson
PO Box 2236
Williston ND 58801

RE: TAC-0053(026), PCN 20772
RAY SRTS-PHASE ii
IEI PROJECT NO.: S13-04-233
WILLIAMS COUNTY

Dear Mr. Peterson:

Thank you for your letter dated September 10, 2014 requesting comments on the proposed roadway improvements within the City of Ray on Score Street, Comfort Street, Fourth Avenue, and Sixth Avenue.

The North Dakota Department of Emergency Services, Division of Homeland Security has no comments on the proposed project.

Should you have any additional questions, please contact me at 701-328-8256.

Sincerely,

Cody Schulz
Disaster Recovery Chief
ND Department of Emergency Services
Division of Homeland Security

Enclosure: Request Letter

Jack Dalrymple
Governor

Major General David Sprynczynatyk
Director - Department of Emergency Services

Greg M. Wilz
Director - Division of Homeland Security

Mike Lynk
Director - Division of State Radio



October 2, 2014

Mr. Dean Petersen, P.E.
Senior Project Engineer
Interstate Engineering, Inc.
P.O. Box 2236
Williston, ND 588012-2236

Re: City of Ray Safe Routes to School – Phase II, Williams County

Dear Mr. Peterson:

This department has reviewed the information concerning the above-referenced project submitted under date of September 10, 2014, with respect to possible environmental impacts.

This department believes that environmental impacts from the proposed construction will be minor and can be controlled by proper construction methods. With respect to construction, we have the following comments:

1. All necessary measures must be taken to minimize fugitive dust emissions created during construction activities. Any complaints that may arise are to be dealt with in an efficient and effective manner.
2. Care is to be taken during construction activity near any water of the state to minimize adverse effects on a water body. This includes minimal disturbance of stream beds and banks to prevent excess siltation, and the replacement and revegetation of any disturbed area as soon as possible after work has been completed. Caution must also be taken to prevent spills of oil and grease that may reach the receiving water from equipment maintenance, and/or the handling of fuels on the site. Guidelines for minimizing degradation to waterways during construction are attached.
3. Projects disturbing one or more acres are required to have a permit to discharge storm water runoff until the site is stabilized by the reestablishment of vegetation or other permanent cover. Further information on the storm water permit may be obtained from the Department's website or by calling the Division of Water Quality (701-328-5210). Also, cities may impose additional requirements and/or specific best management practices for construction affecting their storm drainage system. Check with the local officials to be sure any local storm water management considerations are addressed.
4. Noise from construction activities may have adverse effects on persons who live near the construction area. Noise levels can be minimized by ensuring that construction equipment is

Mr. Dean Petersen

2.

October 2, 2014

equipped with a recommended muffler in good working order. Noise effects can also be minimized by ensuring that construction activities are not conducted during early morning or late evening hours.

The department owns no land in or adjacent to the proposed improvements, nor does it have any projects scheduled in the area. In addition, we believe the proposed activities are consistent with the State Implementation Plan for the Control of Air Pollution for the State of North Dakota.

If you have any questions regarding our comments, please feel free to contact this office.

Sincerely,

A handwritten signature in black ink, appearing to read "L. David Glatt". The signature is written in a cursive, somewhat stylized font.

L. David Glatt, P.E., Chief
Environmental Health Section

LDG:cc
Attach.



Construction and Environmental Disturbance Requirements

These represent the minimum requirements of the North Dakota Department of Health. They ensure that minimal environmental degradation occurs as a result of construction or related work which has the potential to affect the waters of the State of North Dakota. All projects will be designed and implemented to restrict the losses or disturbances of soil, vegetative cover, and pollutants (chemical or biological) from a site.

Soils

Prevent the erosion of exposed soil surfaces and trapping sediments being transported. Examples include, but are not restricted to, sediment dams or berms, diversion dikes, hay bales as erosion checks, riprap, mesh or burlap blankets to hold soil during construction, and immediately establishing vegetative cover on disturbed areas after construction is completed. Fragile and sensitive areas such as wetlands, riparian zones, delicate flora, or land resources will be protected against compaction, vegetation loss, and unnecessary damage.

Surface Waters

All construction which directly or indirectly impacts aquatic systems will be managed to minimize impacts. All attempts will be made to prevent the contamination of water at construction sites from fuel spillage, lubricants, and chemicals, by following safe storage and handling procedures. Stream bank and stream bed disturbances will be controlled to minimize and/or prevent silt movement, nutrient upsurges, plant dislocation, and any physical, chemical, or biological disruption. The use of pesticides or herbicides in or near these systems is forbidden without approval from this Department.

Fill Material

Any fill material placed below the high water mark must be free of top soils, decomposable materials, and persistent synthetic organic compounds (in toxic concentrations). This includes, but is not limited to, asphalt, tires, treated lumber, and construction debris. The Department may require testing of fill materials. All temporary fills must be removed. Debris and solid wastes will be removed from the site and the impacted areas restored as nearly as possible to the original condition.



North Dakota Geological Survey

Edward C. Murphy - State Geologist

Department of Mineral Resources

Lynn D. Helms - Director

North Dakota Industrial Commission

<https://www.dmr.nd.gov/ndgs/>

October 6, 2014

Dean Peterson
Interstate Engineering, Inc.
PO Box 2236
Williston, ND 58801

Dean,

I have reviewed our records to determine if any paleontological sites have been reported from the tracts listed for the TAC-0053(026), PCN 20772, Ray Srts – Phase II, IEI Project No.: S13-04-233, Williams County project.

No fossil sites have been identified in any of the tracts listed.

It is unlikely that paleontological sites will be encountered in the Williams County tracts because those areas are covered with generally unfossiliferous glacial deposits.

Sincerely,

Jeff Person
Paleontologist
North Dakota Geological Survey



North Dakota Department of Transportation

Grant Levi, P.E.
Director

Jack Dalrymple
Governor

October 20, 2014

Dean Peterson
Interstate Engineering
PO Box 2236
Williston ND 58801

PCN: 20772
Project #: TAC-0053(026) Ray Safe Routes to School
RE: CULTURAL RESOURCE STATUS.

Enclosed is a copy of the North Dakota State Historic Preservation Officer (SHPO) stamped letter indicating that the SHPO concurs with my No Historic Properties Affected determination. I recommend no additional cultural resource work prior to construction. If you have any questions regarding this determination, or if any changes in the project develop, please contact me at 701-328-4539.

BOB CHRISTENSEN, ENVIRONMENTAL & TRANSPORTATION SERVICES DIVISION



**STATE
HISTORICAL
SOCIETY
OF NORTH DAKOTA**

Jack Dalrymple
Governor of North Dakota

North Dakota
State Historical Board

Calvin Grinnell
New Town - President

A. Ruric Todd III
Jamestown - Vice President

Margaret Puetz
Bismarck - Secretary

Albert I. Berger
Grand Forks

Gereld Gertholz
Valley City

Diane K. Larson
Bismarck

Chester E. Nelson, Jr.
Bismarck

Sara Otte Coleman
*Director
Tourism Division*

Kelly Schmidt
State Treasurer

Alvin A. Jaeger
Secretary of State

Mark Zimmerman
*Director
Parks and Recreation Department*

Grant Levi
*Director
Department of Transportation*

Merlan E. Paaverud, Jr.
Director

*Accredited by the
American Alliance
of Museums since 1986*

October 8, 2014

Mr. Bob Christensen
Archaeologist, ETS Division
Dept of Transportation
608 East Boulevard Avenue
Bismarck, ND 58505-0700

ND SHPO Ref.: 15-5030, NDDOT TAC-0053(026) PCN 20772, Ray Safe Routes to Schools Phase II, in portions of [T156N R97W Section 9], Ray, Williams County, North Dakota

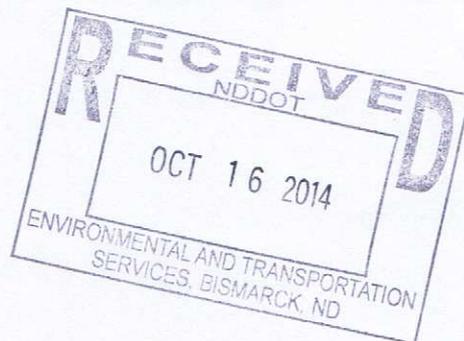
Dear Mr. Christensen,

We reviewed ND SHPO Ref.: 15-5030, NDDOT TAC-0053(026) PCN 20772, Ray Safe Routes to Schools Phase II, in portions of [T156N R97W Section 9], Ray, Williams County, North Dakota and we concur with the determination of "No Historic Properties Affected" for this project provided that it takes place in the location and in the manner described in the documentation and that all borrow comes from an approved source.

Thank you for the opportunity to review this project. If you have any questions please contact Lisa Steckler, Preservation Planner at (701) 328-3577, e-mail lsteckler@nd.gov

Sincerely,

Merlan E. Paaverud, Jr.
State Historic Preservation Officer
(North Dakota)



Attachments:

SECTION 7 DETERMINATION TABLE

Table 1

NDDOT Threatened, Endangered, Proposed, Candidate Species and Critical Habitat Affect Determination Table							
Project: TAC-0053(026)		PCN: 20772	Location: City of Ray, ND		County: Williams		
Species	Listing	Guidance	FHWA Review Required?		Determination		Additional Documentation Included
			Yes	No	Not Present	No Effect	
Interior Least Tern	E	FHWA Review required for work in or along the shoreline of the Missouri River System including reservoirs from April 15 through August 1.		X		X	
Whooping Crane	E	FHWA Review required for work involving above ground utilities or towers, or new guy wires unless lines are buried.		X		X	
Black-footed Ferret	E	FHWA Review required for ground disturbing activities within 100 feet of prairie dog towns of at least 80 acres in size. Projects within the existing right-of-way will not require FHWA review.			X		
Pallid Sturgeon	E	FHWA Review required for work in or along the shoreline of the Missouri River (including reservoirs) and Yellowstone River Systems.		X		X	
Gray Wolf	E	No FHWA Review required		X		X	
Piping Plover	T	FHWA Review required for ground disturbing activities within ½ mile of designated piping plover critical habitat or known nesting sites. See link for piping plover designated critical habitat maps: http://www.fws.gov/mountain-prairie/species/birds/pipingplover/		X		X	
Western Prairie Fringed Orchid	T	FHWA Review required for all ground disturbing activities on non-flooded, undisturbed ground, known habitat, and native prairie. High probability of species in or near the Sheyenne National Grassland.			X		
Dakota Skipper	P	FHWA Review required for work occurring outside of the right of way in high quality native prairie containing a high diversity of wildflowers and grasses.			X		
Poweshiek Skipperling	P	FHWA Review required for work occurring outside of the right of way in undisturbed native tall grass prairie and wet swales.			X		
Northern Long-Eared Bat	P	FHWA Review required for work involving the removal of trees or buildings, ground disturbance in areas with caves, mines, and rock crevices, or work on structures.		X		X	
Rufa Red Knot	P	FHWA Review required for work activities impacting Piping Plover Critical Habitat or sewage lagoons. See link for piping plover designated critical habitat maps: http://www.fws.gov/mountain-prairie/species/birds/pipingplover/		X		X	
Greater Sage Grouse	C	Occur in native sagebrush grasslands where big sagebrush (<i>Artemisia tridentata</i>) is present. (Determination not required for Candidate Species)			X		
Sprague's Pipit	C	Occur in large native short-to-mixed grass prairie patches of approximately 72 acres or greater. (Determination not required for Candidate Species)		X			
Piping Plover Critical Habitat	D	FHWA Review required for ground disturbing activities within ½ mile of designated piping plover critical habitat or known nesting sites. See link for piping plover designated critical habitat maps: http://www.fws.gov/mountain-prairie/species/birds/pipingplover/		X		X	
Poweshiek Skipperling Critical Habitat	P	FHWA Review required for ground disturbing activities within ½ mile of proposed Poweshiek Skipperling critical habitat. See link for Poweshiek Skipperling proposed critical habitat maps: https://www.fws.gov/midwest/endangered/insects/posk/poskPropCHMapUnitsND_SD.html			X		
Dakota Skipper Critical Habitat	P	FHWA Review required for ground disturbing activities within ½ mile of proposed Dakota Skipper critical habitat. See link for Dakota Skipper proposed critical habitat maps: http://www.fws.gov/midwest/endangered/insects/dask/CHmaps/daskNDCHmaps.pdf			X		

Listing Key: E – Endangered T – Threatened P – Proposed C – Candidate D – Designated Critical Habitat

Attachments:

WETLAND DELINEATION REPORT

RAY SRTS- PHASE II

**Project No. TAC-0053(026)
PCN 20772**

**Various Locations
Ray, North Dakota**

Prepared by

**City of Ray
Ray, North Dakota**

**Principal Author:
Interstate Engineering, Inc.**

October 2014

TABLE OF CONTENTS

Description	Page
Table of Contents.....	1
I. Introduction	2
II. Methods	2
III. Results	2
IV. References.....	2
V. Delineator's Credentials	2
Table 1, Wetland Table	3
Exhibit 1 Project Location Map	4
Exhibit 2 Wetland Map.....	5

I. Introduction

The City of Ray in cooperation with the North Dakota Department of Transportation and the Federal Highway Administration, is proposing a Sidewalk – Safe Routes to School project at various locations within the City. **Please refer to Exhibit 1, Project Location Map.** The project consists of the installation of 5' sidewalks. Portions will replace existing deteriorated sidewalks and the remaining is new sidewalk. ADA accessibility issues will be addressed.

This project is expected to be constructed during the 2015 construction season.

Scott Johnston of Interstate Engineering, Inc. conducted an office wetland delineation for the proposed project on October 24, 2014.

II. Methods

The office wetland delineation was conducted in accordance with Section II-03.10.01 of the NDDOT Design Manual. Wetland boundaries within the study area were determined by referencing the National Wetlands Inventory (NWI) layer over aerial photography on the NWI website.

The project area was extended to 100 feet on either side of the sidewalks.

III. Results

No Wetlands were identified within the project area.

Please refer to Table 1, Wetland Table and Exhibit 2, Wetland Map.

IV. References

North Dakota Department of Transportation. 2009. NDDOT Design Manual. Available online: <http://www.dot.nd.gov/manuals/design/designmanual/designmanual.htm>.

National Wetland Inventory (NWI) Data by the US Fish and Wildlife Service
<http://www.fws.gov/wetlands/data/index.html>

V. Delineator's Credentials

Name: Scott Johnston
Education: North Dakota State University – Civil Engineering
Registration: North Dakota Professional Engineer

Table 1, Wetland Table

The office wetland determination for PCN 20772, Project TAC-0053(026), was conducted on October 24, 2014, by Scott Johnston of Interstate Engineering, Inc. The wetland delineations were conducted using a combination of NWI maps and aerial photography, assuming a 200 foot buffer around the project area.

Wetland Number	Location	LONG/LAT (Dec. Deg.)	Cowardin Classification	Wetland Type	Wetland Size (acres)	Wetland Feature
No Wetlands Exist						

Exhibit 1 – Project Location Map

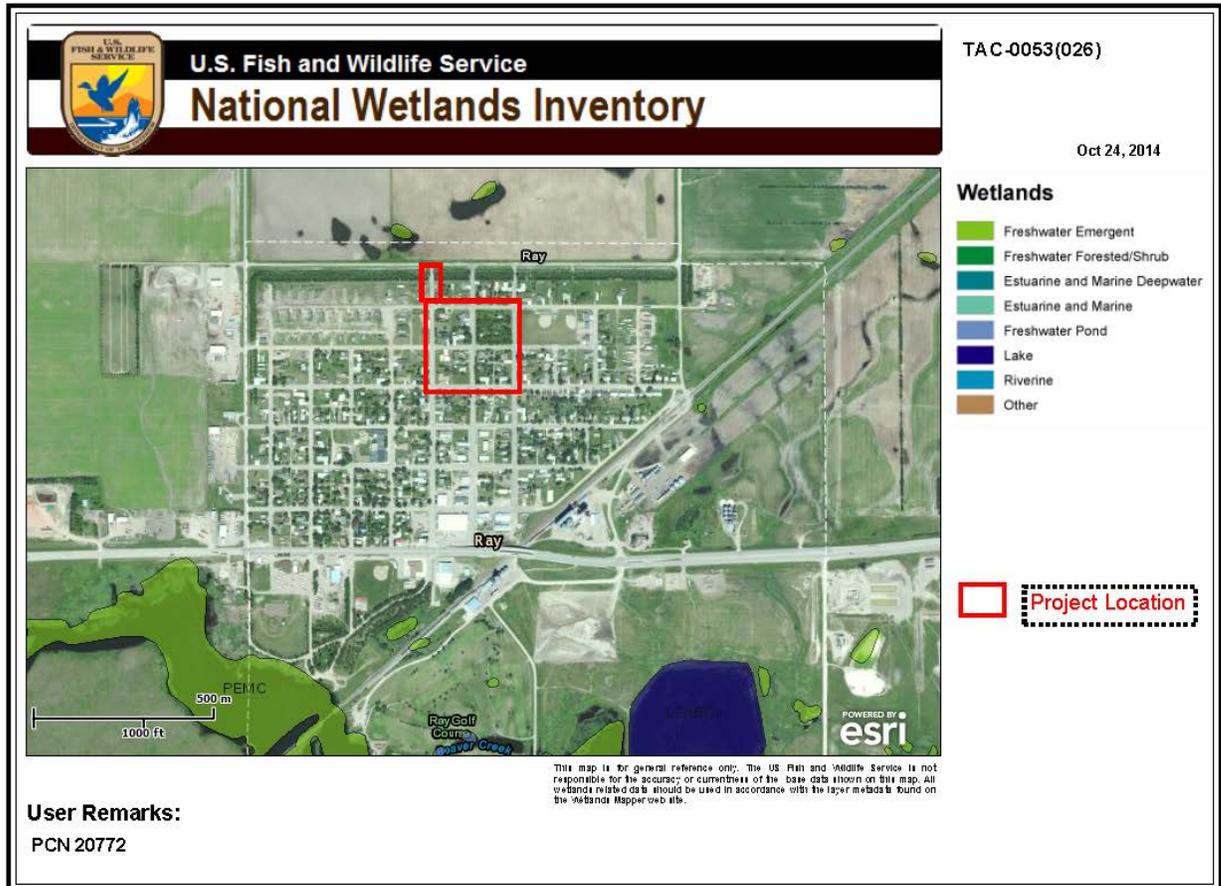


Exhibit 2 – Wetland Map

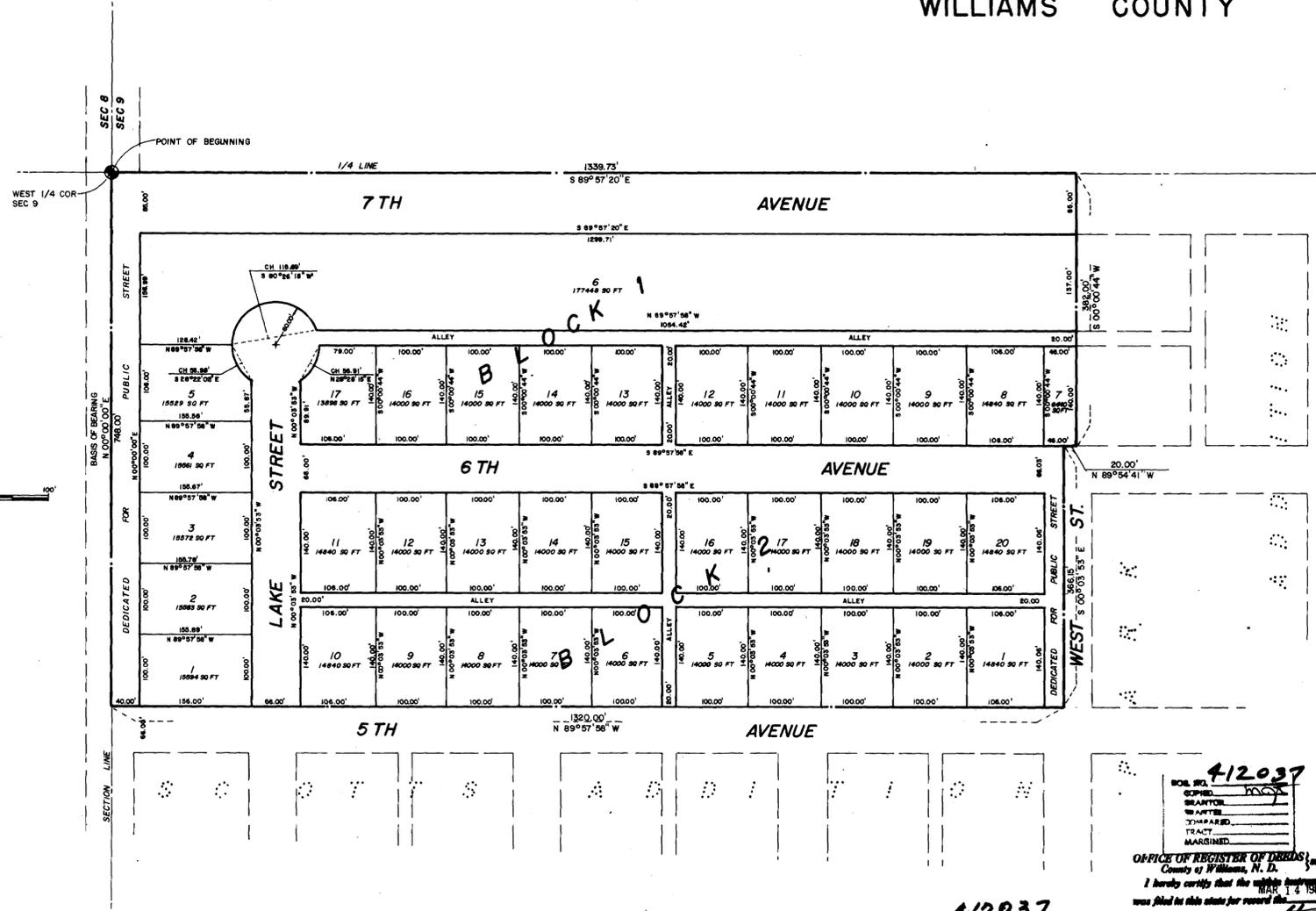


THOMPSON ADDITION

N 1/2 SW 1/4 SEC 9 T156N · R97W

RAY, NORTH DAKOTA

WILLIAMS COUNTY



CURVE DATA

LOT NO.	RADIUS	CENTRAL ANGLE	ARC LENGTH
5	60.00	56° 37' 59"	59.30'
6	60.00	161° 01' 59"	168.63'
17	60.00	56° 37' 16"	59.29'

NOTE: ALL PROPERTY CORNER MONUMENTED BY 1/2" DIAMETER x 24" LONG RE-BAR.

BE IT KNOWN TO ALL MEN BY THESE PRESENTS: That the City of Ray, North Dakota, a Municipal Corporation, Owner and Proprietor of that certain parcel of land described as follows, to-wit:

A parcel of land located in the North 1/2 of the SW 1/4 of Section 9, Township 156 North, Range 97 West of the Fifth Principal Meridian, Williams County, North Dakota, more particularly described as follows: Beginning at the west 1/4 corner of said Section 9; thence South 89° 57' 20" East, along the East-West 1/4 line of said Section 9, a distance of 1339.78 feet to a point; thence South 00° 00' 44" West a distance of 382.00 feet to a point on the north line of 6th Avenue; thence North 89° 54' 41" West along said north line of 6th Avenue a distance of 20.00 feet to a point on the west line of West Street; thence South 00° 03' 53" East along said west line of West Street a distance of 356.15 feet to a point on the north line of 5th Avenue; thence North 89° 57' 58" West along said north line of 5th Avenue a distance of 1320.00 feet to a point on the west line of said Section 9; thence North 00° 00' 00" East along said west line of Section 9 a distance of 748.00 feet to the point of beginning, in accordance with the attached plat, containing 22.84 acres more or less; said parcel includes the west one-half of West Street as vacated by Resolution dated June 21, 1979 and filed with the Williams County Register of Deeds as Document No. 406671, has caused said parcel to be surveyed and platted and hereafter known as the "THOMPSON ADDITION", Ray, North Dakota. The said owner hereby donates and dedicates to public use forever all Avenues, Streets and Alleys shown on the attached plat.

In witness whereof, the above named owner has set his hand this 14 day of March, A.D. 1980.

CITY OF RAY, NORTH DAKOTA

By: Kenneth Lund
Kenneth Lund, President of Board of City Commissioners

Attest: Donald R. Fuoter
Donald R. Fuoter, City Auditor

State of North Dakota }
County of Williams } S.S.

On this 14 day of March, 1980, before me personally appeared Kenneth Lund and Donald R. Fuoter known to me to be the persons who executed the above instrument and acknowledged to me that they executed the same.

My commission expires: Aug 14, 1985

Helen Taylor
Notary Public

I hereby certify that I have surveyed and platted the foregoing described tract of land as "THOMPSON ADDITION", Ray, North Dakota, and the plat of the "THOMPSON ADDITION" is a true and correct plat of said survey. All distances are given in feet and decimals of a foot.

Bryan Beggs
Bryan Beggs, Surveyor

State of North Dakota }
County of Williams } S.S.

On this 18 day of October, 1979, before me personally appeared Bryan Beggs known to me to be the person who executed the above instrument and acknowledged to me that he executed the same.

My commission expires August 14, 1985

Helen Taylor
Helen Taylor, Notary Public

This is to certify that the Planning Board of the City of Ray, North Dakota has examined the attached plat of "THOMPSON ADDITION", Ray, North Dakota, and has authorized its president to attach his signature as approval thereof.

PLANNING BOARD OF THE CITY OF RAY, NORTH DAKOTA

By: Arthur Johnson
Arthur Johnson, President

Attest: Donald R. Fuoter
Donald R. Fuoter, Secretary

State of North Dakota }
County of Williams } S.S.

On this 14 day of March, 1980, before me personally appeared Arthur Johnson and Donald R. Fuoter known to me to be the persons who executed the above instrument and acknowledged to me that they executed the same.

My commission expires: Aug 14, 1985

Helen Taylor
Notary Public

This is to certify that the City Commission of the City of Ray, North Dakota has examined the attached plat of "THOMPSON ADDITION", Ray, North Dakota, and has authorized its President to attach his signature as approval thereof.

BOARD OF CITY COMMISSIONERS, CITY OF RAY, NORTH DAKOTA

By: Kenneth Lund
Kenneth Lund, President

Attest: Donald R. Fuoter
Donald R. Fuoter, City Auditor

State of North Dakota }
County of Williams } S.S.

On this 14 day of March, 1980, before me personally appeared Kenneth Lund and Donald R. Fuoter known to me to be the persons who executed the above instrument and acknowledged to me that they executed the same.

My commission expires: Aug 14, 1985

Helen Taylor
Notary Public

412037

BOOK NO.	
DATE	10/14/80
BY	
INDEXED	
FILED	
RECORDED	
MAINTAINED	

OFFICE OF REGISTER OF DEEDS
County of Williams, N. D.

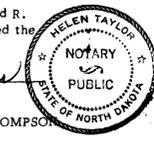
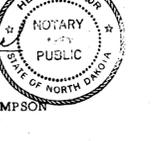
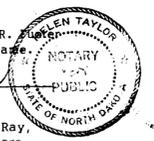
I hereby certify that the within instrument was filed in this state for record this 14 day of March, A. D. 1980.

555
WILLIAMS COUNTY

Recording Fee \$5.00

412037

Mar 14, 1980

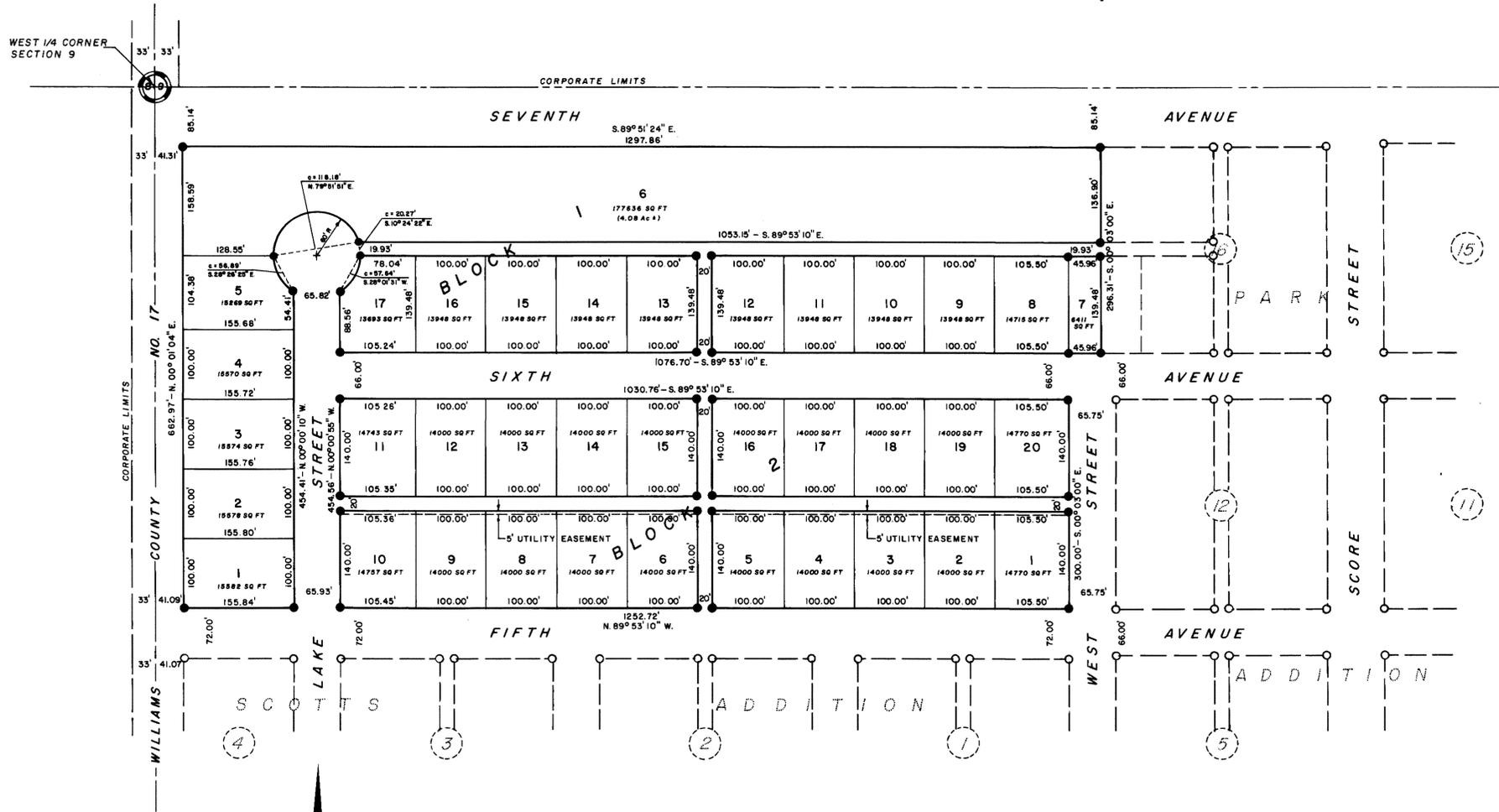


REARRANGEMENT of THOMPSON ADDITION

CITY OF RAY

N 1/2 SW 1/4, SECTION 9, T.156 N. R. 97 W.

WILLIAMS COUNTY, NORTH DAKOTA



PROPRIETORS CERTIFICATE

We, the undersigned, being the owners and mortgage holders of the lands platted hereon, do hereby voluntarily consent to the execution of the Rearrangement of Block 1, Lots 1 thru 17 & Block 2, Lots 1 thru 20, Thompson Addition to the City of Ray, County of Williams, State of North Dakota, and do hereby dedicate the streets, alleys, parks, and public grounds as shown hereon to the public use forever. We agree not to vacate any portions of this plat without consent of the Planning Commission and the City of Ray. By affixing our signatures we agree to said platting. In all events the undersigned hereby excepts and reserves unto themselves as their interests appear, all of the oil, gas and minerals, with right of ingress and egress thereto.

Date: 4/17/90

Louise Dohken
President, City Commission, Ray, ND

Richard Ross
Auditor, City of Ray, ND

Williams County)
North Dakota) s.s.

Be it known that on this 17 day of April, 1990, before me personally appeared Louise Dohken & Richard Ross and acknowledged the execution and signing of the Proprietors Certificate to be his (her, their) own voluntary act and deed.

Date: 4-17-90

David Melland
Notary Public, Williams County, ND

CERTIFICATE OF SURVEY

I, Rodney A. Westrum, a Registered Land Surveyor with Westrum's Land Surveying, Williston, North Dakota, hereby certify that the survey, Rearrangement of Block 1, Lots 1 thru 17 & Block 2, Lots 1 thru 20, Thompson Addition to the City of Ray, Williams County, North Dakota, was done by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of North Dakota.

Date: 5-6-90

Rodney A. Westrum
REGISTERED LAND SURVEYOR
NORTH DAKOTA
Rodney A. Westrum, L.S., 195

Williams County)
North Dakota) s.s.

Be it known that on this 6th day of May, 1990, before me personally appeared Rodney A. Westrum and acknowledged the execution and signing of the Certificate of Registered Land Surveyor to be his (her, their) own voluntary act and deed.

Joseph L. Berce
Notary Public, Williams County, ND

PLANNING COMMISSION APPROVAL

The Rearrangement of Block 1, Lots 1 thru 17 & Block 2, Lots 1 thru 20, Thompson Addition, shown hereon has been approved by the Planning Commission of the City of Ray on April 5, 1990, in accordance with the laws of the State of North Dakota, the ordinances of the City of Ray, and regulations of said Planning Commission. In witness whereof are set the hands and seals of the President of the Planning Commission of the City of Ray.

Date: 4-5-90

Carl Johnson
President

Williams County)
North Dakota) s.s.

Be it known that on this 5 day of April, 1990, before me personally appeared Carl Johnson and acknowledged the execution and signing of the Planning Commission Approval to be his (her, their) own voluntary act and deed.

Date: 4-5-90

David Melland
Notary Public, Williams County, ND

CITY COMMISSION APPROVAL

City Commission Approval of the Rearrangement of Block 1, Lots 1 thru 17 & Block 2, Lots 1 thru 20, Thompson Addition to the City of Ray, is hereby approved and accepted as shown hereon this 5 day of April, 1990, in accordance with the laws of the State of North Dakota, and ordinances of the City of Ray.

Date: 4/7/90

Louise Dohken
President

Richard Ross
Auditor

Williams County)
North Dakota) s.s.

Be it known that on this 17 day of April, 1990, before me personally appeared Louise Dohken & Richard Ross and acknowledged the execution and signing of the City Commission Approval to be his (her, their) own voluntary act and deed.

Date: 4-17-90

David Melland
Notary Public, Williams County, ND

REGISTER OF DEEDS

This, Rearrangement of Block 1, Lots 1 thru 17 & Block 2, Lots 1 thru 20, Thompson Addition to the City of Ray, plat was filed for record in the office of Williams County Register of Deeds, in the State of North Dakota, at 5:45 PM on the 9th day of July, A.D. 1990.

532262

James C. Kelly
Register of Deeds

fee 8.00

Subsequent Real Estate Taxes paid and transfer entered this 3 day of July, 1990.

City of Ray

Incorporated March 9, 1914

Post Office Box 67

Ray, North Dakota 58849-0067

Telephone: 701-568-2204 *** Email: raynd@nccray.net

RESOLUTION 2016-01

RESOLUTION TO APPROPRIATE A 20% MATCH UP TO \$200,000 FROM THE CITY'S GENERAL FUND FOR THE PURPOSE OF THE ND PARKS AND RECREATION'S RECREATIONAL TRAILS PROGRAM (RTP) PROJECT ALONG 7th AVE AND AUTHORIZE THE CITY COMMISSION PRESIDENT AND CITY AUDITOR TO REVIEW AND SIGN THE APPLICATION.

WHEREAS the City of Ray is intending to apply for a grant for the ND Recreational Trails Program (RTP) to construct the recreational trail along 7th Avenue to benefit all citizens of Ray, North Dakota; and

WHEREAS the City Commission for the City of Ray has approved to dedicate to the project 20% up to (\$200,000) from the City's General Fund contingent on award of the RTP grant.

THEREFORE BE IT RESOLVED BY THE BOARD OF CITY COMMISSIONERS OF THE CITY OF RAY, NORTH DAKOTA:

To appropriate a 20% match up to (\$200,000) from the City's General Fund to the project for the ND Recreational Trails Program 7th Ave Shared Use Path Project and authorize the City Commission President and City Auditor to review and sign the application.

Dated January 11, 2016

CITY OF RAY, NORTH DAKOTA

Attest:



Kenneth Munson, President
Board of City Commissioners

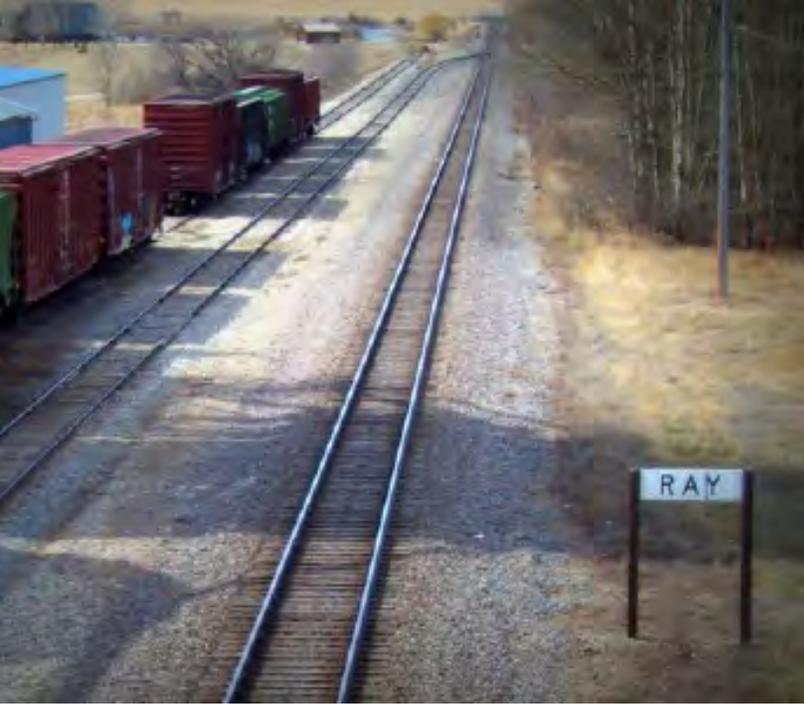
Ronda Rustad
City Auditor

The governing body of the City acted on the foregoing resolution at a properly noticed meeting held in Ray, North Dakota, on January 11, 2016 with the motion for adoption made by Commissioner Rettig and seconded by Commissioner Kupper, and the roll call vote on the motion was as follows:

"Aye" **Kupper, Rettig, and Munson**

"Nay" **None**

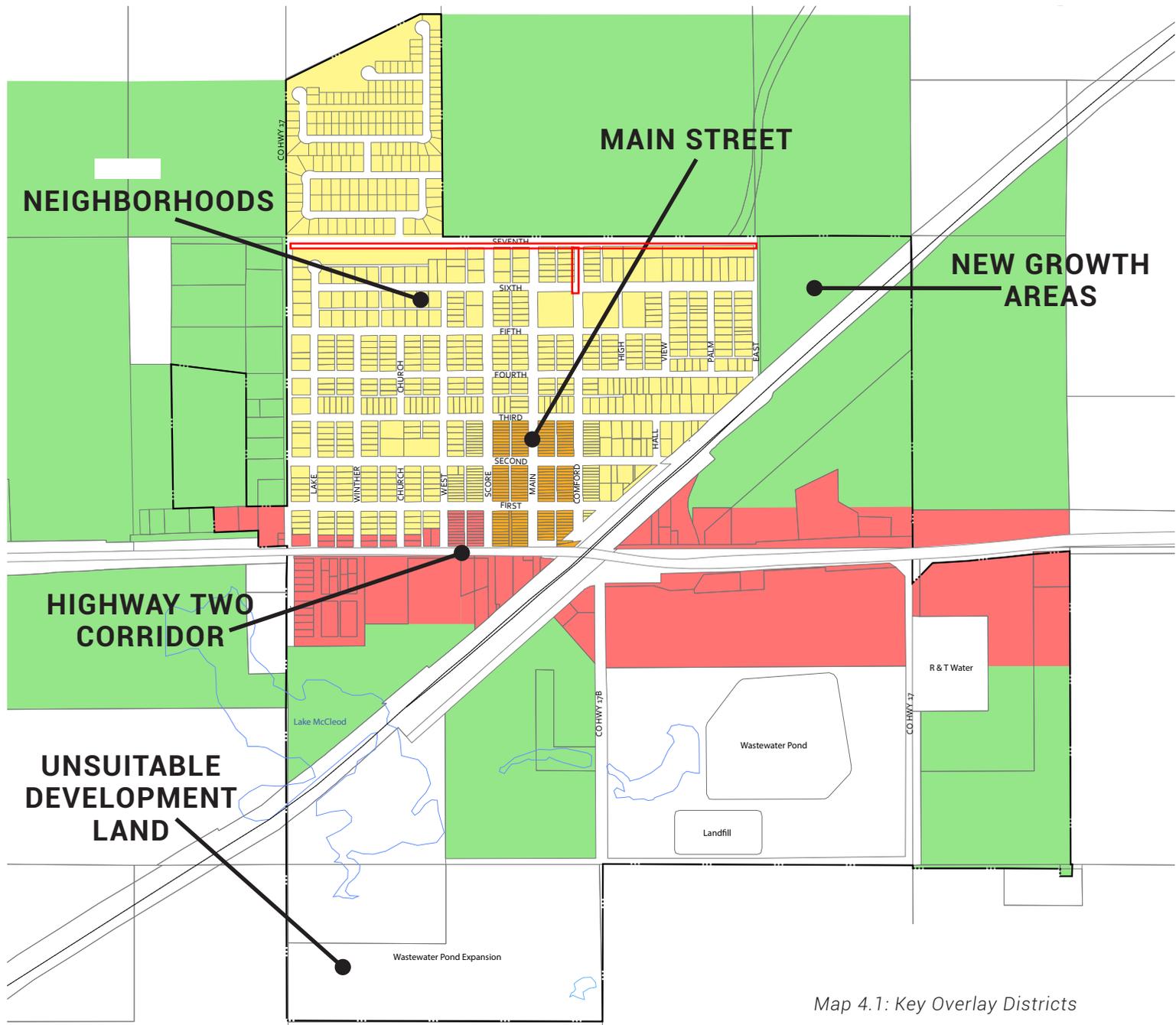
Absent **Liesener**



CITY OF RAY NORTH DAKOTA

2015 COMPREHENSIVE PLAN





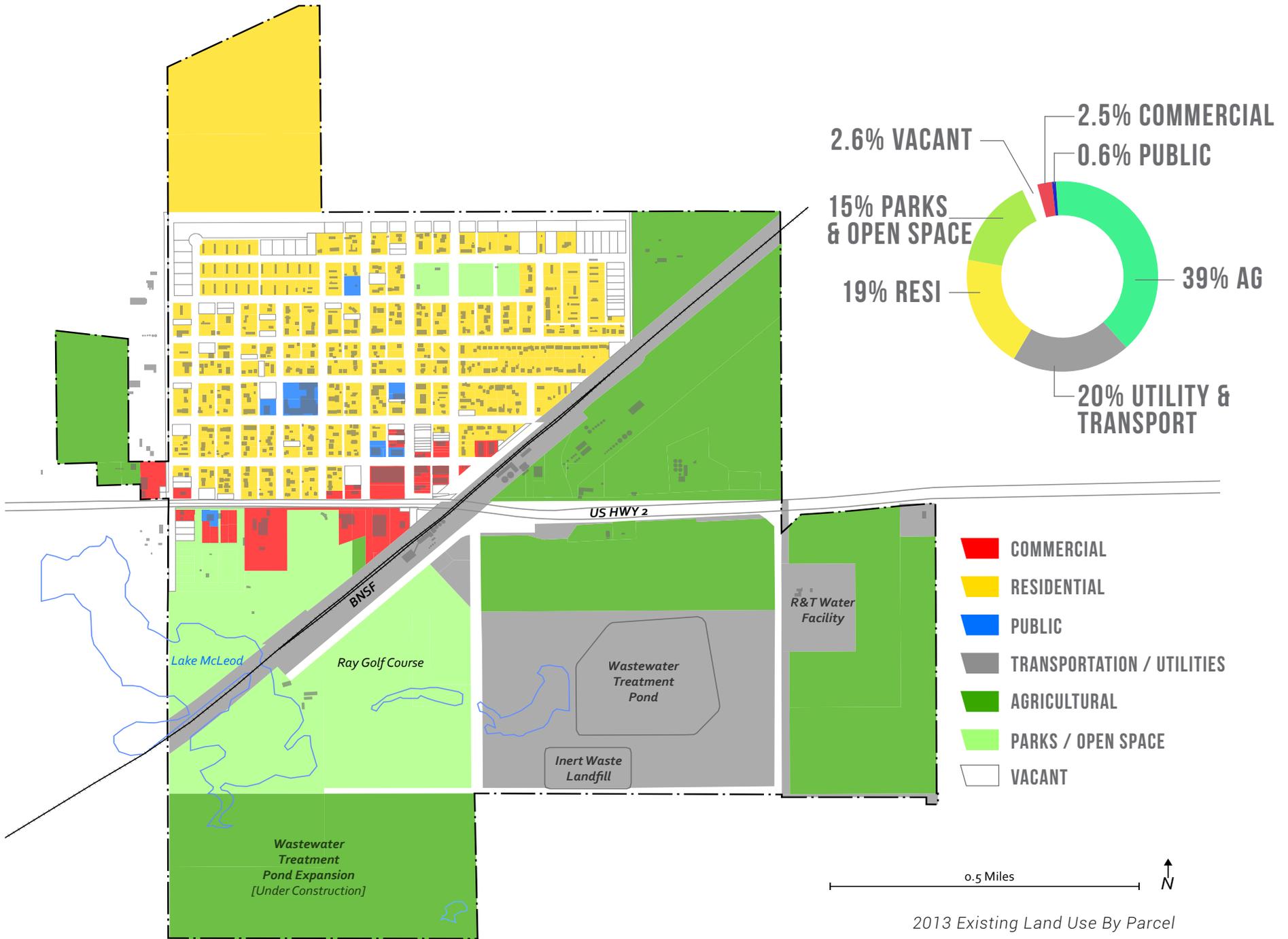
Map 4.1: Key Overlay Districts

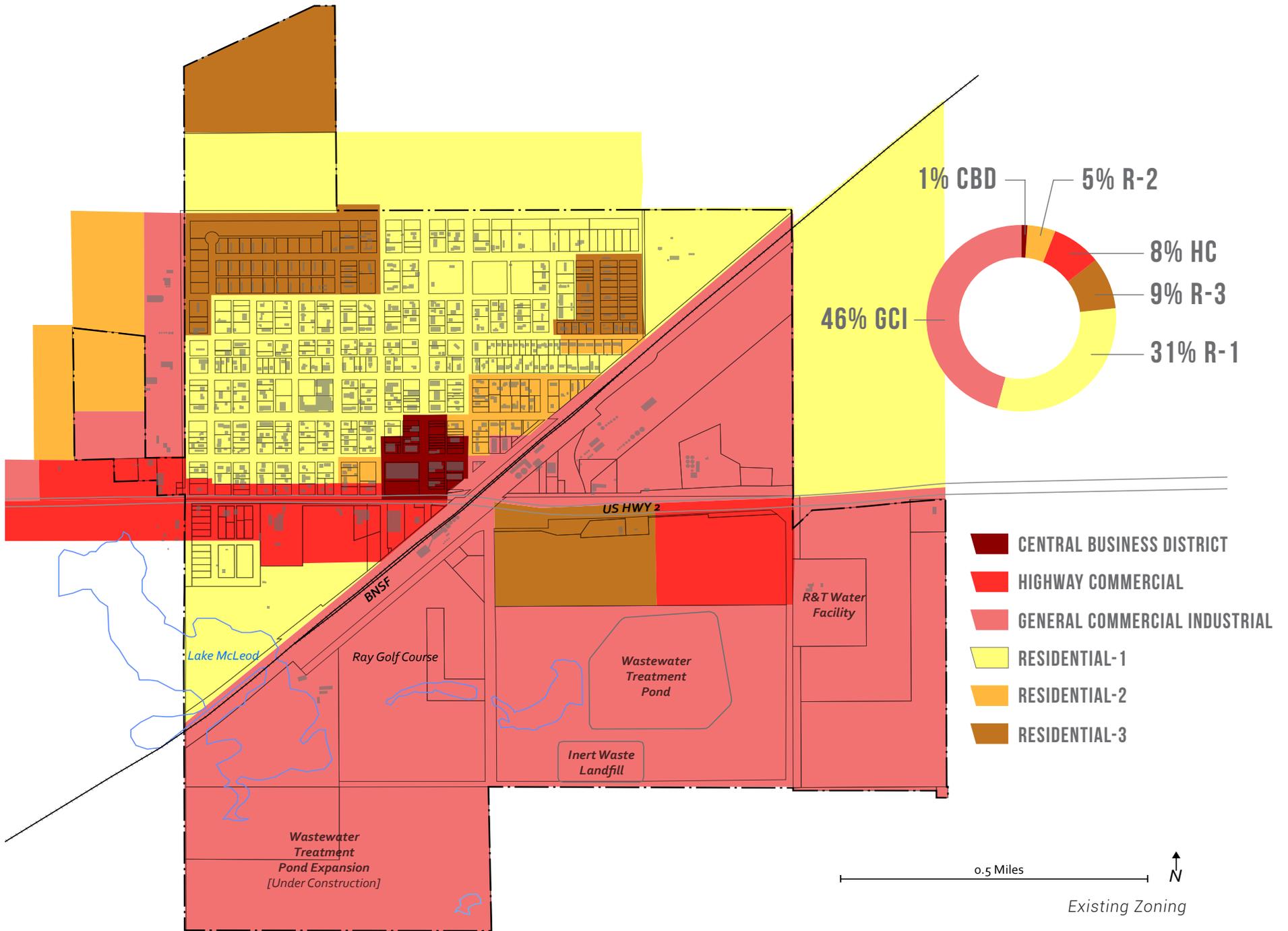


Views of Main Street District



Nationally Registered Historic Ray Opera House (1904) adjacent to a new commercial structure.







Public Infrastructure

Rapid growth experienced by the city in recent years has stretched public infrastructure - water, sewer, roads - to the limit of their originally designed capacity. In addition, Ray's Public Works Department struggles with staffing and financing issues, not unlike many small communities in the Bakken region.

Currently, infrastructure projects are funded through a mixture of property tax levies, production tax funding (see distribution graph to right), sales taxes and loans. With production tax funding guaranteed only one more year, the department is forced to focus primarily on the city's immediate needs while trying to budget for future improvements.

Despite these limited resources, the city has made significant strides in recent years. Over 19,000 feet of watermain line has been replaced, 4 1/2 blocks of new sidewalks installed, construction of a new wastewater treatment system and implementation of a city-wide street maintenance program.

The 2014 construction season will mark the beginning of a three year, city-wide patch and chip sealing project and the start of an 8,500 square foot Sanitary Sewer Collection and Treatment improvement project. In addition, the View Street utility trench will be patched and the city-owned Ray Mall parking lot will undergo major site improvements. Also under consideration is a shared-use path along 7th Avenue.

The following projects are slated for the 2015 or 2016 construction cycle:

- 750,000 gallon water tank
- Main Street asphalt overlay
- Score Street reconstruction
- 10 blocks of new sidewalk
- Reconstruction of 3rd & 4th Avenue

Key Issues:

1. Ensuring enough capacity for long-term growth.
2. Finding sufficient funding to complete the necessary improvement projects.
3. Securing human resources/personnel to complete all slated projects over the long term.



5% DISTRIBUTED TO:

- 1.8% Counties
- 1.4% Schools
- 0.8% Cities
- 0.7% State General Fund
- 0.3% Oil Impact Fund

D | Pedestrian Systems

New development in the Neighborhood District should continue or enhance the pedestrian network by replacing damaged or incomplete sidewalks. Everywhere possible, links should be provided to neighborhood parks and other "natural" areas.

E | Architectural Features and Materials

As with all new construction, residential architectural features should reflect the local agricultural, industrial, pioneer, or western character of the region. Natural materials such as brick, metal, and stone provide emphasis on traditional housing facade materials such as shake shingles, aluminum, wood, porcelain-enameled steel, or composite siding. Colors and materials should resemble those on existing residential structures, avoiding highly-contrasting, highly colorful palettes.

Preferred architectural styles are based on traditional house forms of the Northern Plains. Ranch style, split-level, Victorian, shed style, arts and crafts,

minimal traditional, prairie style, or mid-century modern are common house styles in the region. Larger multi-family apartments and condominiums across the Northern Plains are being built in more contemporary styles.



Examples of preferred materials and textures. Natural textures such as wood, stone, metal, and brick complement traditional building techniques.

Additionally, architectural forms should be used to strategically shield group mailboxes and other similar necessities in multiple family developments.

F | Affordability

Providing adequate affordable housing is a Key Principle of this Comprehensive Plan. Developments that take advantage of federal, state, or local subsidies or incentives to provide affordable housing

will contribute to this goal. Projects that are designed to include a diversity of unit types, sizes, and densities are compliant with the Comprehensive Plan.

G | Sustainability

To the extent possible, new developments should contribute to sustainability goals. Building with locally-sourced materials, constructing energy efficient homes, and using native, drought-tolerant plants in landscaping promotes a sustainable community.

Buildings should be designed for eventual reuse or recycling. The US Environmental Protection Agency estimated that residential construction and demolition produces about 67 million tons of waste, with just about half of that going to landfills. Building with recycled or reusable materials mitigates the potential impacts of construction and demolition waste.

NEW GROWTH AREA DESIGN GUIDELINES

Overview

The New Growth Areas Overlay District comprises the lands inside and outside the corporate boundary that are not part of the original townsite, any additions or platted subdivisions. The official Zoning and Subdivision Regulations currently administer development standards for new construction in this district, and these Design Guidelines illustrate the Comprehensive Plan's vision for new growth. The Future Land Use Map (FLUM) is included here to identify compatible locations for future subdivisions, annexations, or individual projects.

A | Size, Scale and Streetscape

The size and scale of new projects should be compatible with the surrounding uses and structures. In existing industrial areas, for example, a new large-scale structure may be compatible because of existing warehouses or grain elevators, but they are not compatible next to residential areas. Refer to the Future Land Use Map for general development densities.

B | Landscaping and Site Elements

Streetscape and landscape standards are administered in the official Zoning and Subdivision Regulations. Specific streetscape Design Guidelines for New Growth Areas are not warranted beyond those administered in existing land use regulations.

C | Transportation and Parking

Subdivisions in New Growth Areas should be designed to follow the historical street grid. New roads, where possible, should extend out from existing roads and should follow the historical naming conventions. Parking should be developed according to the land use that is assigned to the parcel.



Example of how parcel sizes, black layout, and road extensions should be consistent with existing platted areas and should provide access to future development in New Growth Areas

D | Pedestrian Systems

New Growth Areas should connect to and enhance existing pedestrian pathways in a cohesive network. Everywhere possible, links should be provided to neighborhood parks and other "natural" or recreational areas.

E | Architectural Features and Materials

Architectural design is administered in the official Zoning and Subdivision Regulations. Specific Design Guidelines for New Growth Areas are not warranted beyond those administered in existing land use regulations.

F | Future Land Use

The Future Land Use Map (FLUM) is a guide to properly locating new development in this district. The following future land uses for new growth inside the corporate boundary are described as follows:

1. *Residential*: envisioned to be comparable to the Residential Zone Districts (R-1, R-2 and R-3) in the official Zoning and Subdivision Regulations.

- a. Structure Type: one family detached, two family, multiple family and institutional uses that are complimentary to the district, such as hospitals, senior/assisted living, and religious establishments. Where areas are adjacent to existing residences in the Neighborhood District, the use should be similar.
- b. Residential uses are located according to the Future Land Use Map, and should be sited to take advantage of local services, to promote a compact urban form, and to promote housing affordability.
- c. Design of new subdivisions should comply with the Neighborhood District Design Guidelines.
- d. Parcel Size: 3,500 sq.ft. to 21,840 sq.ft. (half acre)
- e. Density (units per acre): 2-25
2. *Commercial*: envisioned to be comparable to the Highway Commercial (HC) and General Commercial/Industrial (GCI) Zoning District in the official Zoning and Subdivision Regulations. New commercial and industrial activities in this location will complement similar existing uses.
 - a. Structure Type: any general commercial or light industrial structures.
 - b. Commercial uses should be located at the city's western gateway (Highway Two and County Road 17) and along Highway Two. This location is the city's most productive land for commercial development with high visibility, traffic, and accessibility.
 - c. Parcel Size: min 7,000 sq.ft.
 - d. Building Sq.Ft.: max 125,000 sq.ft.
3. *Mixed-Use*: envisioned to be a buffer between future residential and commercial uses. The most appropriate use will be determined as the surrounding area develops. This district extends west from St. Michael's Cemetery, buffering commercial uses to the south along Highway Two from residential uses to the north.
 - a. Structure Type: any general commercial, light industrial or residential structures.
 - b. Parcel Size: Min 7,000 sq.ft.
 - c. Building Sq.Ft.: Max 125,000 sq.ft.